

## 11.5 Kipling Avenue Corridor Secondary Plan

APPROVED BY THE ONTARIO MUNICIPAL BOARD ON SEPTEMBER 30, 2014



## 11.5.1 Development Principles and Objectives

The following principles support the Vision, and represent the fundamental objectives to be considered in the decision-making related to all public and private sector initiatives within the Kipling Avenue Corridor Area:

### **Principle:**

#### **To foster a sense of place.**

11.5.1.1. Development shall contribute to a defined identity and a ‘sense of place’ for the Kipling Avenue Corridor Area, and the area should be planned as a destination in its own right, defined by a strong pedestrian realm and a healthy mix of uses. The following are the objectives to foster a sense of place:

- a. To design a consistent streetscape and a cohesive approach to architecture that draws upon existing assets, including heritage structures and landscapes.
- b. To develop an attractive, pedestrian-oriented street that enhances the public realm with intimately-scaled open spaces, active at-grade uses, gathering spaces, and safe and well-connected boulevards and pathways, resulting in a safe, animated and walkable environment.
- c. To ensure building design is appropriately scaled to frame public spaces and create a comfortable pedestrian environment.
- d. To fill in the gaps in the urban fabric to create a continuous, attractive pedestrian environment and an appropriately-scaled urban form that enhances the character of the street.

#### **To protect heritage resources.**

11.5.1.2. All new development shall respect Kipling’s heritage assets and contribute to its heritage character. The following are the objectives to protect heritage resources:

- a. To ensure that the scale and massing of new development contributes to the heritage character and attributes of the area.
- b. To ensure that open spaces, green corridors and trail connections are considered part of the area’s heritage resources.

#### **To create a supportive transportation network.**

11.5.1.3. The design of the transportation network should support a range of users, including pedestrians, cyclists, public transit and private vehicles. The following are the objectives to create a supportive transportation network:

- a. To develop a street network which is well-connected and is supported by public transit to facilitate vehicular movement which is safe and efficient and reduces traffic congestion.
- b. To ensure that new development supports investment in public transit through increased densities and efficient design.
- c. To design streets and the public realm to ensure safe relationships between pedestrians, cyclists and vehicles.
- d. To provide a safe and well-connected pedestrian and cycling network that facilitates movement throughout the area as well as to surrounding areas in Vaughan.
- e. To mitigate areas of transportation conflict, such as at the railway crossings and along railway corridors, to facilitate improved circulation throughout the area.

**To provide a mix of uses.**

11.5.1.4. The Kipling Corridor area shall accommodate and encourage a mix of uses to support a vibrant community and healthy economy. The following are the objectives to provide a mix of uses:

- a. To establish a mixed-use environment which includes compatible residential, commercial and institutional uses to encourage residents to live and work in the area.
- b. To ensure an appropriate transition between uses and different building types.
- c. To provide a variety of housing types to accommodate a broad demographic population, including a complimentary range of heights, unit types and sizes.

**To enhance a green environment and open space system.**

11.5.1.5. The Kipling Avenue Corridor Area shall be made “green”, including public and private open spaces, streets, pathways and trails. The following are the objectives to enhance a green environment and open space system:

- a. o ensure that green open space corridors (i.e. the Humber River and the Rainbow Creek Corridors) are conserved, enhanced and well-connected.
- b. To ensure that development will be located outside of hazard lands and that the risk to life and property will be minimized to the furthest extend possible.
- c. To provide trails/paths throughout the green corridors, public parks and neighbouring communities that are continuous, accessible, well-linked, and facilitate movement throughout the Kipling Corridor area.



- d. To create a park and open space network that supports an array of both passive and active recreational uses, as well as varying community needs.
- e. To ensure the Fairgrounds remain a key open space amenity for the community, are well-connected to surrounding neighbourhoods, and are designed to encourage year-round activities, while respecting their heritage significance.

### **To achieve a critical mass.**

- 11.5.1.6. The population density on Kipling Avenue should support its role and function as a vibrant neighbourhood within the City of Vaughan. The following is the objective to achieve a critical mass:
  - a. To plan and design the Kipling Avenue area to support intensification and compact urban form, in order to provide residents with a variety of uses and activities within walking distance, support local amenities, increase transit options, and ensure a vibrant street life.

## **11.5.2 Urban Form – Precincts**

This section identifies the desired future character and function of the various distinct precincts within the Kipling Avenue Corridor Area. The definition of the precincts is a result of both the vision generated for Kipling Avenue Corridor, and the analysis generated through the Woodbridge Heritage Conservation District Study.

The character identified for each precinct is prescriptive. The intent is to guide future development and public investments, in a way that serves to: a) conserve significant elements of the existing character; and b) to reinforce the future vision. The precincts are shown on Schedule 11.5.B.

### **Kipling Avenue**

- 11.5.2.1. Kipling Avenue should be:
  - a. A road that maintains a significant function as part of the public realm in the region, providing pedestrian access and views to significant public destinations and amenities, including: Woodbridge Core, the Fairgrounds, and the open spaces associated with the Humber River and the Rainbow Creek.
  - b. A picturesque heritage Avenue, with a significant tree canopy and buildings that front directly onto Kipling. There should be active at-grade uses and buildings should be setback, offering a landscaped front yard.
  - c. A pedestrian friendly street, that provides pedestrian connections to a range of parks, open spaces, trails and walkways, and to commercial and residential areas, throughout the area.

- d. A residential character and scale, with buildings that average 2-3 storeys, and include some commercial and live-work uses.
- e. An area of Vaughan that displays and conserves significant heritage buildings, structures, and landscapes.

### **Woodbridge and Kipling Avenue Intersection**

11.5.2.2. Woodbridge Avenue and Kipling Avenue Intersection should be:

- a. A main Street that extends along Woodbridge Avenue, east to Islington Avenue and west to Kipling Avenue, with a mix of at grade pedestrian oriented uses such as retail, offices and restaurants and a variety of other uses above grade, mostly residential.
- b. A street wall of buildings between 4 and 6 storeys at the intersection.
- c. A welcoming environment where small-scaled storefronts open directly onto the sidewalk and provide pedestrians with a variety of storefronts, which change every few steps.

### **Fairgrounds Area**

11.5.2.3. The Fairground Area should be:

- a. An area to be maintained as a significant open space for the community.
- b. A place that conserves existing significant greenery and tree canopy, that is part of the rural character, and should extend to the surrounding context.
- c. An enhanced open space as a destination to several important pedestrian connections and trails.
- d. Buildings located on site are maximum three storey structures that are characteristic of a rural character and setting.
- e. A centre programmed throughout the year with fairs, events, and functions, (e.g. a farmer's market) that attract residents and visitors from Vaughan and beyond.
- f. Enhanced as an important connection and contributing forest landscape to the natural conservation area directly to the east.
- g. Porter Avenue, as well as new public connections around the perimeter, are designed gateways and treated as significant elements in the overall open space system of the area.

### **Rainbow Creek Neighbourhood North**

- 11.5.2.4. Rainbow Creek Neighbourhood North should be:
- a. A new neighbourhood that is intimately connected to both Kipling Avenue and the open spaces surrounding Rainbow Creek, through a network of pedestrian connections and streets.
  - b. An area of mostly residential uses.
  - c. A neighbourhood that can accommodate a significant residential population, through higher density developments that are adjacent to significant green areas.
  - d. An area in which all streets are welcoming to pedestrians, with active grade related uses, and ample landscaping.

### **Rainbow Creek Neighbourhood South - Central and South West Areas**

- 11.5.2.5. Rainbow Creek Neighbourhood South provides a transition in building scale and height towards the lower scale residences on Woodbridge Avenue and Kipling Avenue, with mid-density development.

### **Rainbow Creek Neighbourhood South - South East Area**

The concept plan for these lands contemplates a block townhouse development with a minimum 8 metre central pedestrian sidewalk and landscaped area.

- 11.5.2.6. The following are general minimum policy requirements for the development of these lands:
- a. The lands must develop comprehensively, considering the best future development scenario for the easterly adjacent properties along Kipling Avenue and the westerly industrial properties along the river corridor, providing opportunities for increased north/south and east/west pedestrian circulation, and connections to Kipling Avenue, Woodbridge Avenue and the river valley.
  - b. Notwithstanding policy 9.2.1.1, the permitted residential uses for these lands, with the exception of the contributing heritage industrial building, include block townhouses, stacked townhouses, street townhouses, and semi-detached dwellings, with a maximum FSI of 0.70.
  - c. The development of these lands must be respectful of the contributing heritage industrial building at the end of Burton's Lane, as identified in Map 11.5.N. The recommended use for the heritage building include small scale, grade related neighbourhood amenity uses, such as daycare, service commercial, or art studios.

- d. The development of these lands must allow for a public pedestrian connection that is visually and physically accessible from Woodbridge Avenue and the Rainbow
- e. Creek Neighbourhood South, to Parkside Drive and the Rainbow Creek Neighbourhood North. The pedestrian connection should take the form of a generous landscaped promenade between the townhouse developments, as a
- f. shared neighbourhood amenity and public “greenway”. Any proposed townhouse development must front the promenade.
- g. The central promenade, where it is located between residential development, should include a continuous minimum 1.5 metre sidewalk and landscaped area which together should form the minimum 8 metre promenade.
- h. The minimum 1.5 metre sidewalk must be constructed within the first phase of the Low Density Residential Neighbourhood development.
- i. The promenade and heritage building should remain clearly visible from Woodbridge Avenue, and a historic recognition of the heritage building’s significance and the old C.P. Rail line should be considered as part of the promenade open space fronting Woodbridge.
- j. Vehicular access to the residential units must be located at the rear, through a rear lane access.

### **Phasing of Development for the Rainbow Creek Neighbourhood**

Currently, the industrial properties within the study area are considered stable, and development of the Rainbow Creek precinct, as envisioned in the plan, provide the best opportunity within the Study Area to create a complete neighbourhood. Build-out of the

industrial lands, therefore, may be a long term phased scenario and dependent on future residential development pressures.

11.5.2.7. Phasing of Development for the Rainbow Creek Neighbourhood is subject to the following policies:

- a. Partial build-out of the neighbourhood north and south is expected and should be achieved through a comprehensive planning process in order to ensure that the character and function of this precinct is achieved.
- b. The successful build-out of this neighbourhood should happen as a phased scenario and is dependent on the collaboration of all landowners in terms of implementing the necessary infrastructure, such as roads and parks, in a form that serves to benefit the overall plan in the short and long term and does not restrict future development opportunities.

- c. Issues related to road access and connections for example, that are key to good future development should be resolved prior to site plan approval.
- d. Land owners may be required to enter into Developer Group Agreements prior to having a development application considered. This will ensure that the front ending costs and land dedication for desirable infrastructure such as key road connections and parks are achieved and landowners of early phase development can be compensated in the future.

### **11.5.3 Block Pattern, Street Network and Linkages**

- 11.5.3.1. The block pattern and street network of Map 11.5.C and Map 11.5.D defines a pattern, grid and hierarchy of streets, and linkages to guide the siting and orientation of future developments and development blocks. For the most part, the plan reinforces the existing structure of blocks and streets. It is mainly along the western side, between the Rainbow Creek and the railway that the need for a new block pattern and street network is identified. As development opportunities arise within the Corridor, achieving permeable block patterns and a fine network of vehicular and pedestrian connections is essential. The pattern, grid and hierarchy of streets, as illustrated in Map 11.5.D, should be implemented through the development process.
- 11.5.3.2. The City encourages the consolidation of land parcels to facilitate comprehensive development particularly in the Rainbow Creek neighbourhoods. In particular the City shall be satisfied that:
  - a. good block patterns, street connections and pedestrian linkages are established to achieve the goals of the vision;
  - b. development proposals consider the comprehensive requirements of the neighbourhood and adjacent lands in establishing road and pedestrian connections;
  - c. streets and blocks are configured to provide building frontages and addresses to the street and encourage parking access at the rear via a rear lane;
  - d. opportunities to increase public access and linkages to parks and open space amenities are achieved as part of good block development;
  - e. private streets and laneways are discouraged, but if allowed, development should provide public pedestrian access, connect to other streets or laneways and be visually appealing and pedestrian friendly; and,
  - f. to consolidate open space to create a more substantial public amenity where this opportunity arises.
- 11.5.3.3. The following sections describe the recommended character, function and hierarchy of new and existing streets within the study area. Further detail and description is

given for each street category within the Corridor in Appendix A: Streetscape Master Plan – Detailed Streetscape Characteristics.

### **Primary Streets**

- 11.5.3.4. Primary Streets are mixed-use commercial/residential streets and are considered main access roads within the street network.
- 11.5.3.5. Primary streets should function as two way connections with R.O.W.'s from 20 metres to 26 metres, accommodate various modes of transportation such as on street parking, transit and bicycles, and be provided with a higher quality streetscape treatment.
- 11.5.3.6. The following policies apply to Kipling Avenue North and South:
- a. Kipling Avenue North and South should function as the main primary street within the Corridor;
  - b. A 26 metre R.O.W., should be maintained for Kipling Avenue north and south. The protection of this R.O.W. width is beneficial for the Avenue in terms of providing opportunities to increase the street tree canopy, and enforce the “green character” prescribed for Kipling Avenue and the Corridor in general;
  - c. The R.O.W. width should accommodate a double row of street trees and a generous pedestrian zone within the boulevard, along the majority of Kipling Avenue;
  - d. The 3.0 metre building setback beyond the R.O.W. should allow for future built form to be more in keeping with the existing heritage building setbacks and provides an additional opportunity for private landscape enhancements;
  - e. The heritage character of this street should be emphasized and enhanced, and the streetscape designed to attract a greater pedestrian presence, as per policy 11.5.2.1; and,
  - f. Additional landscape enhancements such as double street tree planting, may not be feasible at pinch points where existing heritage buildings fall within the 26 metre R.O.W.
- 11.5.3.7. The following policies apply to Kipling Avenue South:
- a. Kipling Avenue, south of Woodbridge Avenue, should have a wider roadway width, with additional travel lanes to accommodate the volume of traffic entering the avenue from Highway 7.
  - b. Bike lanes should continue to run south along Kipling Avenue to Highway 7.

- c. The east bike lane is envisioned to have a dedicated curb edge or rumble strip to the adjacent travel lane to make peak hour on-street parking possible on the east side of the avenue, while protecting the bike lane.
- d. The widened roadway width will allow for only a single row of street tree planting within a 4.5m boulevard.

11.5.3.8. The following policies apply to Woodbridge Avenue East:

- a. Woodbridge Avenue, east of Kipling Avenue, should function in the same way as Kipling Avenue in terms of creating an attractive pedestrian oriented environment, and supporting an enhanced commercial presence and character.
- b. Woodbridge Avenue, between Kipling Avenue and Islington, should maintain a 20m R.O.W. because of the location of existing heritage contributing buildings. There are a significant amount of buildings along this avenue that are considered contributing to the heritage character of the avenue and the area, many of which have minimal or zero setbacks to the 20 metre R.O.W. These buildings should remain in situ, which affects the feasibility of the 26 metre R.O.W. allowance.
- c. The recommendations of the Woodbridge HCD Study is such that, Woodbridge Avenue at the commercial core should reflect the tight, pedestrian oriented character of the existing 20 metre R.O.W. street cross section.
- d. Although the stretch of Woodbridge Avenue, from Islington to Kipling Avenue is envisioned as a future marked bike route in the City of Vaughan Bike Master Plan, incorporating bike lanes would affect having on-street parking as part of the Woodbridge Avenue cross section.
- e. On-street parking should be encouraged along Woodbridge Avenue as a key element to enable existing and future businesses to flourish and is necessary to the success of the commercial context of the area.
- f. In terms of providing bike access, Woodbridge Avenue, from Islington to Kipling Avenue, should be considered under the “Class 3 Facility: Signed Only Routes” category as identified in the Bike Master Plan, which defines an on-street signed bicycle route used to “form a connection to the bike network”, (see section 4.4.3 of the City of Vaughan’s Pedestrian and Bicycle Master Plan). The existing and future traffic volumes for Woodbridge Avenue are considered to be moderate and Woodbridge Avenue is envisioned to have slower moving traffic, as it is viewed as a pedestrian priority area. This would make a signed bike route manageable along Woodbridge Avenue.

11.5.3.9. The following policies apply to Woodbridge Avenue West:

- a. The protected 26 metre R.O.W is feasible on Woodbridge Avenue, west of Kipling Avenue and should be maintained, which allows for enhanced streetscaping within the boulevard, and should function in the same way as

Kipling Avenue in terms of creating an attractive pedestrian oriented environment.

## **Secondary Streets**

- 11.5.3.10. Secondary Streets should function similarly to primary streets, but are mainly residential in character. These streets should also function as two way connections with a minimum 20 metre R.O.W.
- 11.5.3.11. Secondary Streets should be given a higher quality streetscape treatment and allow for bike lanes and on-street parking.
- 11.5.3.12. The following policies apply to Parkside Drive:
  - a. Parkside Drive is envisioned as a new secondary north/south street that runs parallel to Kipling Avenue, mainly servicing the proposed Rainbow Creek neighbourhood north. Parkside Drive enables the creation of new frontage to both the Rainbow Creek Valley public open space system, and to new development blocks that support a High-Rise Residential neighbourhood.
  - b. This street should function as the key vehicular spine for the new neighbourhood and the main connection to Kipling Avenue and the river valley.
  - c. This should be treated as a grand street, with lush greenery, and ample space for pedestrian interaction.
  - d. As many points of access to Parkside Drive from Kipling Avenue should be achieved to increase the development potential for the industrial lands, as well as provide opportunities to redevelop existing adjacent properties.
  - e. The design of Parkside Drive shall include a high-quality pedestrian environment that links buildings, amenities and open spaces and provides east-west connections to the river valley and Kipling Avenue;
  - f. The design of Parkside Drive shall include wider sidewalks (2.5 metre) on both sides, that can accommodate additional street furniture, lighting, and public amenities associated with the proposed park;
  - g. The design of Parkside Drive shall include a 3 metre setbacks to accommodate additional landscaping and front yards for grade related residential units; and
  - h. The design of Parkside Drive shall include parking on both sides of the street, for visitors and park users.



### **Local Connector Streets**

- 11.5.3.13. The Local Connector Streets provide key east/west connections to Kipling Avenue and to the new Rainbow Creek neighbourhood.
- 11.5.3.14. These streets may vary in character depending on the allowable right-of-way, but should function typically in the same way as secondary streets.
- 11.5.3.15. The following policies apply to Rainbow Creek Road and Industry Avenue:
- a. “Rainbow Creek Road” and “Industry Avenue” are two new east/west local street connectors that link Kipling Avenue to Parkside Drive.
  - b. Rainbow Creek Road, which crosses the rail corridor, should function as a key northern gateway to the new Rainbow Creek neighbourhood and as a connection to Meeting House Road. It is envisioned as a two way 20 metre R.O.W. with the same characteristics as Parkside Drive.
  - c. Industry Avenue, which is south of the rail corridor, should provide a two way connection to Kipling Avenue and the new neighbourhood. The right-of-way for this new street is restricted to 17.5 metre because of its adjacency to a heritage building, and provides only a single sided sidewalk condition.
  - d. The City may consider reduced pavement widths for this local road to accommodate additional sidewalk facilities, provided they are satisfied that vehicular accessibility to the new neighbourhood and the location of utilities are not compromised.

### **Special Character Streets**

- 11.5.3.16. The Special Character Street exists along two east-west connections that lead off from the intersection of Kipling Avenue and the railway.
- 11.5.3.17. The following policies apply to both Porter Avenue East and West:
- a. Porter Avenue East should continue to provide east bound access to the Fairgrounds.
  - b. Porter Avenue West is a new westbound street that should provide new connection to the Rainbow Creek open space system and residential neighbourhood.
  - c. The Special Character Street should be characterized by high quality landscaping, an emphasis on the pedestrian environment, and its function as a gateway to significant urban spaces.

- d. Porter Avenue East should signalize and celebrate an entrance to the Fairgrounds.
- e. Porter Avenue West should function as the main pedestrian draw towards new public amenities, open spaces, and residences that are part of the Rainbow Creek neighbourhood development.
- f. Key aspects of the design include:
  - i. a double row of trees for Porter Avenue East, and enhanced landscaping on both sides of the street;
  - ii. a double row of street trees where the R.O.W. allows, along Porter Avenue West;
  - iii. wide sidewalks, with enhanced paving treatment;
  - iv. additional street furnishing and lighting;
  - v. designed gateway elements on Kipling Avenue; and,
  - vi. clear open views, to the valleys east and west.

11.5.3.18. The following policies apply to Porter Avenue East as a Special Character Street:

- a. Porter Avenue East is a local connector and should function as the main gateway to the Fairgrounds.
- b. This is a two way street with a 20 metre R.O.W. and should be primarily pedestrian oriented, providing opportunities for on street parking and new building frontages that support a pedestrian priority environment.
- c. This street should provide additional parking opportunities for the Fairgrounds.
- d. This a Special Character Street and should be given a high quality streetscape treatment.

11.5.3.19. The following polices apply to Porter Avenue West as a Special Character Street:

- a. Porter Avenue West is considered a local connector street within the street network although it can only function as a one way out to Kipling Avenue due to C.P. Railway setback and sight-line restrictions at that location.
- b. Even though this street serves as a minor connector, Porter Avenue West is key to the realization of the “Kipling Avenue Junction”, and should function as a major east/west pedestrian and bicycle link from Rainbow Creek to the Fairgrounds and beyond.

- c. This is a Special Character Street that should be given the same high quality streetscape treatment as Porter Avenue East.

### **Residential Streets**

- 11.5.3.20. Residential Streets within the study area are neighbourhood streets that carry lower volumes of vehicular traffic.
- 11.5.3.21. These streets should be pedestrian oriented in character and should cater primarily to the safe movement of pedestrians as a priority.

### **Laneways**

- 11.5.3.22. Laneways should mainly provide vehicular access to parking and services.
- 11.5.3.23. They should be organized as internal connections within blocks that link to main streets, and should also be designed as opportunities for increased pedestrian linkages throughout the Corridor.
- 11.5.3.24. The following policies apply to Public Laneways:
  - a. Public Laneways should function primarily as vehicular connections, but should also be considered as opportunities for pedestrian linkages to Kipling Avenue and to new areas of development.
  - b. They should be designed with a special or enhanced streetscape treatment.
  - c. Historically Burton's Lane, just north of Woodbridge Avenue, was the main connection to the Toronto, Grey and Bruce Railway Station, and to some of the first industrial buildings, such as the old Woodbridge Farmer's Co-operative Company.
  - d. Burton's Lane should function as a key public access lane from Kipling Avenue to the Rainbow Creek neighbourhood.
  - e. Burton's Lane should be enhanced with higher quality materials and streetscaping to signify its heritage significance.
- 11.5.3.25. The following policies apply to Private Laneways:
  - a. Private laneways are considered contributing to the overall street network as they provide pedestrian linkages throughout the area and should be designed to be pedestrian friendly, accessible, visible, and safe.
  - b. Private laneways should be located at the rear of buildings only as a means of accessing and connecting to parking facilities and services.

- c. Furthermore, they should provide a continuous pedestrian connection to adjacent streets or open spaces.

### **The Pedestrian Priority Nodes**

- 11.5.3.26. The Pedestrian Priority Nodes are the key intersections along Kipling Avenue and should function as a focus for pedestrian activity.
- 11.5.3.27. In terms of street character and transportation hierarchy, pedestrians take priority within these nodes.
- 11.5.3.28. A high quality streetscape treatment should be given to these key intersections including special paving, lighting, street configuration, landscaping and street furniture.
- 11.5.3.29. The following policies apply to Woodbridge Avenue and Kipling Avenue Intersection:
  - a. This is the main intersection along Kipling Avenue and should function as the main gateway to the Corridor.
  - b. Special consideration should be given to achieving a pedestrian oriented environment by reducing lane widths, providing on-street parking and bike lanes, increasing the opportunity for additional landscaping, and art; and integrating adjacent public open spaces.
- 11.5.3.30. The following policies apply to Porter Avenue East and West - The Junction:
  - a. These streets, together, should create a pedestrian node, referred to as the “Junction”, which is a key east/west pedestrian connection along Kipling Avenue, and at the centre of the Corridor.
  - b. The same special consideration should be given to this node as the Woodbridge/Kipling node in terms of achieving a pedestrian oriented environment.

### **11.5.4 Pedestrian Priority Zones**

Various areas along Kipling Avenue have been identified as places where the pedestrian and cyclists are considered to be a critical part of the infrastructure and place making, and where a balance between the role of the automobile and the requirements of pedestrians and cyclists, particularly in terms of traffic movement, should be established. The Pedestrian Priority Zones, shown on Map 11.5.D are established at key areas along Kipling Avenue in order to provide safe and comfortable walking and cycling environments. These zones are areas where higher levels of pedestrian activity and amenity are anticipated, such as the Woodbridge/Kipling Avenue Gateway or the Rail Junction Public Square; or where

street crossing and safe pedestrian and bicycle movement across Kipling Avenue is a priority. The Pedestrian Priority Zone establishes a first preference to pedestrians, second preference to cyclists and third preference to automobiles.

- 11.5.4.1. In general, Kipling Avenue should no longer function only as a through street. The integration of the Pedestrian Priority Zones should be used as a method of street calming and traffic management.
- 11.5.4.2. Areas where the Pedestrian Priority Zone has been established, priority must be given to the pedestrian in terms of access, circulation, capacity and amenity.
- 11.5.4.3. Transportation infrastructure within these zones should accommodate transit and bicycle facilities and amenities where possible such as transit stops, shelters, bike racks, and safe passenger drop-off areas.
- 11.5.4.4. The street crossings should have a high level of design consideration in terms of traffic calming measures such as: signalization, clearly defined boundaries, marked crosswalks and signage, and special paving materials.
- 11.5.4.5. Traffic calming measures such as pedestrian signalization should be considered at the Meeting House Road intersection. Truck and pedestrian conflicts are currently a problem at this intersection.
- 11.5.4.6. Within these zones, the location of vehicular service entrances, and parking access and egress should be located away from the pedestrian realm to avoid interfering with pedestrian circulation and activity.
- 11.5.4.7. Special design considerations must be given to the ease of access and circulation for the handicapped within these zones.
- 11.5.4.8. Porter Avenue and Porter Avenue West are special pedestrian oriented streets within the Pedestrian Priority Zones and should be designed with a higher emphasis on landscaping and pedestrian amenity, such as special paving materials, roll curb edges to extend the boundaries of the public realm, pedestrian scaled lighting and special signage.
- 11.5.4.9. Open Space areas within these zones should be used for amenities that cater to the pedestrian and cyclist and should not be used for surface parking.
- 11.5.4.10. A strategy to replace the surface parking of the United Church site should be considered to extend the boundaries of the Rail Junction Public Square and include the church within the public square context. A future below grade shared parking scenario should be considered with adjacent properties as development around the church occurs.

- 11.5.4.11. Surface parking should not be permitted within, or directly adjacent to the Pedestrian Priority Zones, but opportunities for on-street parking should be encouraged and identified.

### **11.5.5 Heritage Conservation**

- 11.5.5.1. All new development shall respect Kipling's heritage assets and contribute to its heritage character. The objectives for the conservation of Kipling's heritage assets are:
- a. To ensure that the scale and massing of new development contributes to the heritage character and attributes of the area.
  - b. To ensure that open spaces, green corridors and trail connections are considered part of the area's heritage resources.

### **11.5.6 Parks and Open Spaces**

The park and open space framework defines the desired future character and function of the various components that form the open space system, including parks, enhanced streets, trails, bike paths and civic uses. The intent of this framework is to guide private developments and public investments towards the creation of a consistent and inter-connected system of public and private open spaces and to identify opportunities to maximize the amount of open space available. Parks are shown on Schedule 11.5.F.

- 11.5.6.1. The total park and open space requirement in relation to total proposed development is based on:
- a. 1022 units.
  - b. Park requirement based on  $1\text{ha}/300\text{units} = 3.4\text{ ha (8.4 acres)}$
  - c. Total park space proposed in the Secondary Plan area = 1.75 ha (4.33 acres), see Map 11.5.F: Proposed Parks, Parkettes and Public Squares.
- 11.5.6.2. Additional open space opportunities include:
- a. Trails
  - b. Fairgrounds, heritage landscape
  - c. Neighbourhood parks
  - d. Neighbourhood school open space amenities
  - e. TRCA lands - River Corridors and Conservation Lands

f. Public realm enhancements - streets and nodes

- 11.5.6.3. The total amount of parkland required in accordance with the proposed Vision is approximately 3.0 ha (7 acres). Currently, there are no opportunities along Kipling Avenue to achieve parks of a neighbourhood park scale or greater. The existing Fire Hall site would permit a small neighbourhood park at the south end of the corridor, and the industrial lands. The industrial lands provide the best opportunity to achieve a significant open space amenity and one that is central to the corridor.
- 11.5.6.4. There are additional open space opportunities that will not offset the parkland requirement of 1ha/300units, however, they should be considered as opportunities to increase the overall open space amenity for the Corridor. The development of a significant and connected trail system throughout the Corridor and better use of the river corridors provide significant open space opportunities. The City, in conjunction with the TRCA, should consider locations within the conservation lands where additional recreational uses can be accommodated without detriment to the natural system.
- 11.5.6.5. As well, there are opportunities to use a portion of the Fairgrounds land for recreational purposes, especially at the northern end where trail connections to the east and west river corridors can be integrated. Other recreational opportunities include the utilization of the neighbouring schools such as the Christian District High School, which have significant open space and play facilities. The existing parks and community centres are also within a five to ten minute walking distance from Kipling Avenue. These open space opportunities are described in further detail in the following sections.

## 11.5.7 Open Spaces

The Kipling Avenue Corridor is characteristically defined by three major open space systems that constitute the larger mass of the open space network: the Rainbow Creek, the Humber River Corridor, and the Fairgrounds. These larger open space systems should play a major role in achieving a connected, accessible open space network as well as in achieving a connected neighbourhood fabric. The neighbourhood parks, parkettes, plazas and public squares should provide a fine grain layering of open spaces that support and enhance the larger system. Kipling Avenue should function as the central north-south spine that connects all aspects of the open space network.

- 11.5.7.1. Open spaces are shown on Map 11.5.E.
- 11.5.7.2. The following policies apply to Rainbow Creek and Humber River:
- a. Publicly accessible areas and trails within the valley corridors should be easily accessible, connected and clearly defined in order to protect the natural flora and fauna of the conservation lands.

- b. The trail system within the valleys should be well connected throughout, providing opportunities to partake in views, key features and destinations within the system as well as providing opportunities to commute to the various adjacent neighbourhoods.
- c. The industrial lands located within the TRCA floodplain boundaries shall form part of the Rainbow Creek public open space, allowing for increased opportunity for new trails and additional open space amenity as described in the following guideline.
- d. New open spaces within the industrial lands should be considered and enhanced as opportunities to increase the publicly accessible neighbourhood park amenities with compatible recreational uses such as a children's play area connected to the trails.
- e. The existing mature tree canopy should be protected and enhanced at every opportunity, especially along Rainbow Creek where residential development is being proposed. Other significant opportunities for enhancement exist around the Fairgrounds and should be considered in conjunction with enhancing the trail system. Any form of enhancement to the natural system within TRCA jurisdiction must be undertaken in accordance with the TRCA Act.
- f. Trail heads and access points to the valley corridors should be clearly defined and signed from Kipling Avenue.
- g. Bike parking racks should be provided in the new parks, at the entrance to the valley trails, and along Kipling Avenue where possible.

11.5.7.3. The following policies apply to Kipling Avenue:

- a. Kipling Avenue should be the focus for beautification and enhancement of the public realm, with key pedestrian nodes such as the Woodbridge/Kipling Avenue intersection and the Kipling Avenue Junction as the main focus of social activity. The Avenue should be defined by a cross section that is organized to accommodate at a minimum, enhanced landscaping and street furniture, a comfortable pedestrian walking zone and a zone for retail and café spill-over.
- b. This would include a defined palette of materials that contribute to creating an overall cohesiveness and character for the Corridor such as street signage, benches, garbage and recycling, and pedestrian scaled lighting.
- c. Materials used along the Corridor should be of high quality to provide longevity to the streetscape and should allow ease of maintenance and servicing. The style of materials should strengthen the identity for the Corridor and complement the existing heritage fabric.
- d. Where possible, new streetscaping should integrate any existing mature trees within the streetscape master plan and enhance all heritage landscape frontages with additional planting.



11.5.7.4. The following policies apply to The Fairgrounds:

- a. A new landscape design should be applied to the Fairgrounds that maintains the existing open field, enhances the existing uses, and accommodates additional programmed activity.
- b. A new landscape should also define the racetrack boundaries and the Fairgrounds edges, enhance the surrounding existing mature canopy and conservation lands, and focus views to the river valley and to the eastern Woodbridge neighbourhoods.
- c. A continuous trail connection should be accommodated around the perimeter
- d. of the Fairgrounds and connect to the surrounding neighbourhoods and to
- e. Kipling Avenue.
- f. Porter Avenue, which is the “gateway” entrance to the Fairgrounds, should be clearly defined by a double alleé of street trees and enhanced landscaping including pedestrian scaled lighting and signage. This streetscape should also be applied to the new “Porter Avenue West” to create a continuous east/west landscaped connection of open spaces.

11.5.7.5. The following policies apply to Neighbourhood Parks, Parkettes and Public Squares:

- a. The proposed and existing neighbourhood parks, parkettes, and public squares within the Corridor should be designed, programmed and furnished to meet the day-to-day open space needs of the community
- b. Existing parks, parkettes, and public squares should be enhanced where possible, with additional trees, landscaping and furnishings that are in keeping with the overall vision and palate of materials for the Corridor.
- c. There should be a hierarchy of use and programming established for the parks in order to ensure a range of uses to meet the needs of a variety of users and age groups.
- d. New neighbourhood parks, parkettes, and public squares should be planned with enough flexibility to accommodate special or additional programming, such as community gatherings, and special events.
- e. New neighbourhood parks, and parkettes, should be designed with high quality materials, innovative design, and the newest technology in play facilities.
- f. Any park along Kipling Avenue should be visible and publicly accessible with clear views in and throughout and have an open face to Kipling Avenue.
- g. Parks should have adequate lighting and signage for ease of use and access.

- h. The public squares should be designed to support social events, street related events and activities and day-to-day gathering. These spaces should be the forum for art related activity and art display.
- i. To ensure maximum utilization and presence in the community, the public squares should be fronted by animated uses with a high level of transparency, such as
- j. restaurants, cafés and market venues.
- k. Extending the pavement treatment from the public square onto the street can give the space further prominence while delineating an extended space that can be occasionally utilized for large scale events.

11.5.7.6. The following policies apply to Pedestrian Nodes:

- a. There are two key pedestrian nodes along Kipling Avenue: the Kipling/Woodbridge Avenue Intersection and the “Kipling Avenue Junction”.
- b. Pedestrian nodes occur at key junctures along Kipling Avenue and function as gateways to other surrounding open spaces and to the neighbourhoods east and west of Kipling Avenue.
- c. Pedestrian nodes provide places to meet,congregate and increase social activity and pedestrian amenities. The nodes provide opportunities to celebrate the heritage character of the Corridor and are the focus for high quality enhanced streetscaping treatments.

## 11.5.8 Open Space Connections

11.5.8.1. Open Space Connections are shown on Map 11.5.G.

11.5.8.2. A connected system of parks and open space should service all aspects of the Corridor and the surrounding neighbourhoods and should be made publicly accessible by a completed trail network.

### Enhanced Streetscapes

11.5.8.3. All new and existing neighbourhood streets should provide continuous streetscaping to contribute to the overall tree canopy.

11.5.8.4. Special enhanced landscape treatments should be applied to streets that function as gateways and/or are identified as pedestrian priority zones, such as Porter Road and an enhanced Porter Road West.

### **Pedestrian Connections**

- 11.5.8.5. Pedestrian connections should be provided within the public realm to ensure ease of access and permeability throughout the Corridor, such as midway connections through blocks and through all public parks and open spaces.

### **Pedestrian Crosswalks**

- 11.5.8.6. Safety measures should be applied at key pedestrian crossings along Kipling Avenue and Parkside Drive, including signal lights and enhanced streetscape treatments and road paving materials.
- 11.5.8.7. Pedestrian crosswalk materials and design should be in keeping with the overall Streetscape Master Plan Vision for Kipling Avenue.
- 11.5.8.8. Recommended locations for pedestrian crosswalks are at the intersections of Kipling Avenue and Meeting House Road, Kipling Avenue and Porter Avenue, Porter Avenue West and the new Parkside Drive, and Woodbridge Avenue and Kipling Avenue.

### **Bike and Pedestrian Trails**

- 11.5.8.9. All development shall implement the requirements of the City of Vaughan's Pedestrian and Bicycle Master Plan.
- 11.5.8.10. The new trail system proposed in the Vision should expand and enhance the Pedestrian and Bicycle Master Plan.
- 11.5.8.11. All existing and proposed parks and public open spaces within the Corridor should be connected to, and made accessible by, the new trail network.
- 11.5.8.12. The bike trail along Kipling Avenue should be incorporated in the Streetscape Master Plan for Kipling Avenue in the form of bike lanes, as proposed in the City of Vaughan's Pedestrian and Bicycle Master Plan.
- 11.5.8.13. Where possible, Neighbourhood and Valley trails should be accessible from Kipling Avenue and access points should be demarcated with trail signage that is in keeping with the overall vision for signage and wayfinding.

### **11.5.9 Landmark Sites, Gateways and Nodes**

Landmarks, nodes and gateways, as shown on Map 11.5.H, are the special places and features within the Corridor and are meant to define and enhance the character and identity of the Corridor as well as provide specific functions at strategic locations.

## **Landmark Sites/Signature Architecture**

- 11.5.9.1. The Landmark Sites, as identified in Map 11.5.H include both, locations for signature architecture and open spaces. Collectively, these sites should help define the Kipling Avenue Corridor as a distinct and special place within Woodbridge.
- 11.5.9.2. Obtaining LEED building standards and incorporating green building technologies for new developments/buildings will also contribute to the area as a distinct and special place.
- 11.5.9.3. The following policies apply to Open Space Landmarks:
  - a. Most of the open space landmark sites are heritage features and are dispersed along the Corridor. These sites constitute a variety of forms from memorial sites at a small scale to the Fairgrounds and Rainbow Creek at a much larger scale. Most of the sites are not immediately visible from Kipling Avenue and provide an element of surprise and discovery along the Corridor. The accessibility to, and visual presence of, such sites should be enhanced through measures such as discrete signage, trail connections, enhanced landscaping, visible art elements, and mapping of the sites posted at the nodes along Kipling Avenue.
  - b. The open space landmark sites should be given special treatment in terms of quality of landscaping, materials and design that are unique to each open space.
  - c. Opportunities to enhance the distinct characteristics of the existing landmark sites should be considered as a first priority such as, enhanced landscaping and additional tree planting for the mature canopy of the northern gateway landmark site, which can then be augmented by an architectural feature within the landscape as a terminus to the Avenue.
- 11.5.9.4. The following policies apply to Signature Architecture Landmarks:
  - a. Most of the significant built form landmarks are focused around the new neighbourhood development west of Kipling Avenue and the entrance gateway buildings at Porter Avenue West; as well as the Woodbridge/Kipling Gateway.
  - b. The design of the buildings should reinforce the distinct character of the area and showcase a high quality of architecture and materials.
  - c. The collective of new buildings proposed within the new Parkside Drive neighbourhood should together, display a landmark quality and stature that will contribute to the enhancement of the river valley and define the scenic character of Parkside Drive.
  - d. The built-form should demonstrate high quality materials and innovative design that allows for physical and visual integration and interplay of the valley landscape and that provides a human scale comfort for pedestrians.

- e. Built-form elements should include having numerous openings and mid-block pedestrian connections to the valley; visually accessible courtyards and gardens; as well as terraces, balconies, and green roofs as outdoor amenity space and as opportunities to landscape at each above grade level.
- f. The same should apply for the buildings proposed for the Woodbridge/Kipling Gateway, which collectively, should display a language of architectural features that defines and demarcates the node as a key focal point.
- g. Buildings should also work individually to provide a diversity and interest in architectural form that establishes a pleasing and comfortable built form and public realm.
- h. The buildings at the Woodbridge/Kipling Gateway should also contribute to the visual interest as seen from the Woodbridge Core with architectural elements and function to orient people towards the Core.
- i. Individually, each building should contribute to the architectural diversity and interest along Parkside Drive, and to the visual interest of the valley corridor and public open spaces, with architectural features that provide identifiable visual landmarks from the neighbourhood west.
- j. All buildings along the valley edge should therefore front the street as well as the valley and back building uses such as servicing and parking entrances should be visually hidden from the valley, parks and pedestrian trails.
- k. Buildings at the new Porter Avenue West gateway should display a corner articulation as a focal point to the entrance of Parkside Drive and to frame views west toward the river valley.
- l. Buildings at the “Rail Junction Public Square” should enhance the existing heritage United Church building and tower and other heritage properties, and should contribute with visually prominent elements to demarcate the public square along the Avenue and to frame views toward the river valley west.
- m. Buildings around the public square should have architectural features that enhance the quality of public areas, reinforce neighbourhood and district identity, and provide a sense of enclosure and orientation for the public realm.

11.5.9.5. The following policies apply to Primary Gateways:

- a. The primary gateways are located at the Woodbridge and Kipling Avenue Intersection, at the “Rail Junction Public Square”, and at Kipling Avenue North at the bend of the road. The primary gateways serve to define the boundaries of the Corridor in terms of establishing an identifiable character, place, and quality. Each gateway is considered as a distinct node with unique characteristics.
- b. Woodbridge Avenue and Kipling Avenue Gateway: A framing of higher density development, signature architectural buildings, at-grade pedestrian activity, and a

new pedestrian public square define this Gateway. Buildings should be articulated at the corners with primary entrances facing the street, with a high level of streetscaping to identify the gateway as a distinct place and to organize pedestrian and vehicular movement.

- c. The Kipling Avenue Junction: This gateway celebrates the entrance to the new Parkside Drive neighbourhood and highlights the presence of two major open space systems within the Kipling Avenue Corridor. A new landscaped public square with flexible space to service the new neighbourhood and existing community, an enhanced streetscape treatment along Kipling Avenue, the Public Square, and the Porter Avenue entrances (east and west) defines the Kipling Avenue Junction Gateway.
- d. Kipling Avenue North: The gateway treatment for the northern end of Kipling Avenue should represent the distinct mature landscape character of the Corridor, as the gateway is situated in the most treed area of the Avenue and at a key northern trailhead. A gateway feature can be represented in the form of enhanced landscaping or art and should incorporate trail signage and way-finding information.

11.5.9.6. The following policies apply to Secondary Gateways:

- a. The secondary gateways are located at Meeting House Road and at the intersection of the new Parkside Drive and Porter Avenue West.
- b. The secondary gateways are key entryways into the neighbourhoods west and east of Kipling and are identified as places that require enhanced streetscaping at the intersections such as special landscaping and paving materials, enhanced frontages to corner heritage buildings, and signalized pedestrian crosswalks.

11.5.9.7. The following policies apply to Potential Public Art Sites:

- a. Public art can help to establish a unique identity and can contribute to enhancing the quality of the public realm in ways that conventional streetscape elements cannot. Key locations for public art that have been identified include the primary and secondary gateway nodes, especially those that are defined with heritage sites, new and existing parks, parkettes and public squares. There are also opportunities to display public art in the Fairgrounds, such as in the form of a gateway treatment or as a focal point to guide views out towards the valley.
- b. Public art may include memorials, statues, water features, or individual art installations and can be incorporated in street furniture, signage and wayfinding, and should be located at visually prominent sites, public squares, parks, gateways, along sidewalks, at trail heads and/or in association with public and heritage buildings.
- c. Art installations associated with heritage sites should contribute to and not take away from the heritage character, architecture and quality of the heritage building and landscape.

- d. Art installations associated with the valley corridors should contribute to and not take away from the quality and character of the landscape and natural system, should not interrupt key views and vistas, and should not be disruptive of the natural flora and fauna of the valley lands.
- e. Public art installations should serve as accents to the Corridor, as orienting devices for moving about, and as focal points in public open spaces.
- f. The scale of art installations should correspond to the visual prominence of the site.

11.5.9.8. The following policies apply to Transit and Trail Node:

- a. Three nodes along the Avenue, as illustrated in Map 11.5.H, are identified as key locations for information kiosks for pedestrian and bike trails as well as bus routes, and should be incorporated in a signage and wayfinding strategy for the Avenue.

### 11.5.10 Public Realm Enhancements

11.5.10.1. Public realm enhancements and increased parkland can be achieved through land use designations of Parks or public squares, or public realm improvements will be negotiated through the City's development processes and can be achieved through:

- a. The Private Sector - via private development agreements in the form of parkland dedication, where such lands will be developed and conveyed to the City of Vaughan for parkland or public realm improvement purposes.
- b. The Public Sector - via ongoing municipal capital improvement programs aimed at parkland acquisition, parkland development, and streetscape improvements as identified in the Streetscape Master Plan, as well as other future streetscape initiatives.

### 11.5.11 General Land Use Policies

The Kipling Avenue area is intended to become a “complete” neighbourhood, with a “fine-grain” mix of land uses located within walking distance. The area is also intended to accommodate a different range of housing types, which will derive in part from different densities.

11.5.11.1. Map 11.5.A delineates the boundaries and location of the following land use designations:

- a. **Low-Rise Mixed-Use**
- b. **Low-Rise Residential**
- c. **Mid-Rise Mixed-Use**

- d. **Mid-Rise Residential**
- e. **High-Rise Mixed-Use**
- f. **High-Rise Residential**
- g. **Private Open Space – The Fairgrounds**
- h. **Parks**
- i. **Natural Areas**

11.5.11.2. General land use policies that apply to all land use designations are:

- a. Commercial uses may include retail and office uses, and in neighbourhood designations may include ancillary commercial uses such as a corner store. Commercial uses, particularly retail should be located at grade level and face the street.
- b. Civic uses shall be limited to the following: government offices, libraries, community centres and places of worship.
- c. Commercial uses are only permitted as part of a mixed-use building.
- d. Structures that include commercial uses must be a minimum of 2 storeys and a minimum of 7 meters high.
- e. Parking areas and parking structures, with the exception of those located within the Low-Rise Residential designation, must not be visible from the street. Wherever possible parking lots should not be visible from public parks, parkettes or public squares.
- f. Drive-through facilities, gasoline service stations, gas bars and uses that require outdoor storage, are not permitted.

11.5.11.3. For the purpose of calculating density, Floor Space Index (FSI) shall be the Gross Floor Area of a development divided by the Gross Site Area. The deployment of allowable density must not result in buildings exceeding the maximum number of storeys or the maximum lot coverage.

11.5.11.4. The area included in the calculation of residential density shall include areas of a property reserved for setback requirements, the land for the buildings, private roads and driveways, parking areas and landscaping, amenity areas related to the specific development, or any other part of the property where development is restricted through policy or design, but shall exclude all other lands such as natural conservation areas.



### 11.5.12 Low-Rise Mixed-Use

- 11.5.12.1. The Low-Rise Mixed-Use designation represents a low scale, mixed-use area, with a healthy residential base that provides amenities and ancillary retail for adjacent neighbourhoods. This area is focused around Kipling Avenue, north and south of Woodbridge Avenue. The intent is to allow uses along Kipling Avenue to evolve and adapt, as the demand for commercial and retail space changes. This area is ideal for grade-related live-work units that may change overtime, as new residential neighbourhoods are established, towards the west.
- 11.5.12.2. The maximum building height within Low-Rise Mixed-Use areas shall be 11.0 meters (3 storeys).
- 11.5.12.3. The minimum building height within Low-Rise Mixed-Use areas shall be 8.5 meters (2 storeys).
- 11.5.12.4. The maximum lot coverage within Low-Rise Mixed-Use areas shall be 50%.
- 11.5.12.5. The minimum setback in Low-Rise Mixed-Use areas shall be 3.0 meters.
- 11.5.12.6. The density within Low-Rise Mixed-Use areas shall be 0.6 to 1.0 FSI.
- 11.5.12.7. Notwithstanding policy 9.2.1.1, the following broad variety of residential, commercial, recreational and open space uses will support the heritage character of Kipling Avenue and a healthy pedestrian environment:
  - a. residential units and live-work units;
  - b. small scale, grade related and service commercial, business and professional office, daycares, and eating establishments, as part of a mixed-use building with a maximum gross floor area of 100 square metres; and,
  - c. parks and public squares.
- 11.5.12.8. Notwithstanding policy 9.2.2.2 the following building types are permitted, pursuant to Section 9.2.3:
  - a. Detached House
  - b. Semi-detached House
  - c. Townhouses
  - d. Low-Rise Buildings

- 11.5.12.9. Kipling Avenue has the highest concentration of heritage resources within the Corridor. Notwithstanding the allowable height and setbacks, all new construction must abide by the guidelines for transitioning from heritage resources outlined in policy 11.5.1.2.

### **11.5.13 Low-Rise Residential A**

- 11.5.13.1. The Low-Rise Residential A designation represents development within the healthy neighbourhood fabric.
- 11.5.13.2. The maximum building height within Low-Rise Residential A areas shall be 8.5 meters (2 storeys).
- 11.5.13.3. The minimum building height within Low-Rise Residential A areas shall be 4.0 meters (1 storey).
- 11.5.13.4. The maximum lot coverage within Low-Rise Residential A areas shall be 40%.
- 11.5.13.5. The minimum setback within Low-Rise Residential A areas shall be 4.5 meters.
- 11.5.13.6. The maximum density within Low-Rise Residential A areas shall be 0.5 FSI.
- 11.5.13.7. Notwithstanding policy 9.2.2.1, the permitted building types within Low-Rise Residential A areas identified on Map 11.5.A are Detached Houses and Semi-Detached Houses pursuant to Section 9.2.3.
- 11.5.13.8. Notwithstanding 11.5.13.7 above, permitted building types on the frontage facing Gate House Court shall include only Detached Houses to maintain the character of the existing residential court.
- 11.5.13.9. The area identified as “Deferral Area” on Map 11.5.A is subject to an ongoing review of a proposal to have the site considered for a Performing Arts Centre through a public/private partnership arrangement with the City and prospective landowner. In the interim, the underlying designation of Low-Rise Residential A will continue to apply to the lands.

### **11.5.14 Low-Rise Residential B**

- 11.5.14.1. The Low-Rise Residential B designation represents a transition between the Mid-Rise Residential areas and areas with a lower-scaled residential use.
- 11.5.14.2. The maximum building height within Low-Rise Residential B areas shall be 11.0 meters (3 storeys).

- 11.5.14.3. The minimum building height within Low-Rise Residential B areas shall be 8.5 meters (2 storeys).
- 11.5.14.4. The maximum lot coverage within Low-Rise Residential B areas shall be 60%.
- 11.5.14.5. The minimum setback Low-Rise Residential B areas shall be 3.5 meters.
- 11.5.14.6. The maximum density within Low -Rise Residential B areas shall be 0.7 FSI.
- 11.5.14.7. Notwithstanding policy 9.2.2.1, the following uses are permitted in the areas designated Low-Rise Residential B on Map 11.5.A:
  - a. residential units, limited to the following:
    - i. Semi-Detached Houses
    - ii. Townhouses
    - iii. Stacked Townhouses
  - b. one convenience retail store located at grade, with a maximum GFA of 100 square metres as part of a residential building; and,
  - c. parkettes and public squares.
- 11.5.14.8. Notwithstanding the foregoing, the lands immediately west of the 5 storey building located at 8026 Kipling Avenue are restricted to a maximum height of 2 storeys (8.5 metre) for residential units.
- 11.5.14.9. Permitted uses for the heritage building include small scale, grade related neighbourhood amenity uses, such as a daycare, service commercial, or art studios.

### **11.5.15 Low-Rise Residential C**

- 11.5.15.1. The Low-Rise Residential C designation represents a transition between the Mid-Rise Residential areas and areas with a lower-scaled residential use.
- 11.5.15.2. The maximum building height within Low-Rise Residential C areas shall be 13.0 meters (4 storeys).
- 11.5.15.3. The minimum building height within Low-Rise Residential C areas shall be 8.5 meters (2 storeys).
- 11.5.15.4. The maximum lot coverage within Low-Rise Residential C areas shall be 80%.

- 11.5.15.5. The minimum setback in Low-Rise Residential C areas shall be 4.5 meters.
- 11.5.15.6. The maximum density within Low-Rise Residential C areas shall be 2.0 FSI.
- 11.5.15.7. Notwithstanding policy 9.2.2.1, the following uses are permitted in the areas designated Low-Rise Residential C on Map 11.5.A:
  - a. residential uses.
  - b. one convenience retail store located at grade, with a maximum GFA of 100 square metres as part of a residential building; and,
  - c. parkettes and public squares.
- 11.5.15.8. Notwithstanding 9.2.2.1. the following building types are permitted, pursuant to Section 9.2.3:
  - a. Semi-Detached House;
  - b. Townhouses;
  - c. Stacked Townhouses; and,
  - d. Low-Rise Buildings

#### **11.5.16 Mid-Rise Mixed-Use:**

- 11.5.16.1. The Mid-Rise Mixed-Use designation represents the primary retail and commercial centre for the area. It is also intended to concentrate a significant residential population. It is focused at the intersection of Woodbridge Avenue and Kipling Avenue. The intent of the Plan is to establish an animated, pedestrian-oriented, and compact urban environment.
- 11.5.16.2. The maximum building height within Mid-Rise Mixed-Use areas shall be a 13 metre (4 storeys) podium with 19 metre (6 storeys) maximum, stepping back on a 45 degree angular plane from the podium.
- 11.5.16.3. The minimum building height within Mid-Rise Mixed-Use areas shall be 8.5 meters (2 storeys).
- 11.5.16.4. The maximum lot coverage within Mid-Rise Mixed-Use areas shall be 60%.
- 11.5.16.5. The minimum setback Mid-Rise Mixed-Use areas shall be 0.0 meters (consider 3.0 meters when additional public realm is required).
- 11.5.16.6. The maximum density within Mid-Rise Mixed-Use areas shall be 3.0 FSI.

- 11.5.16.7. Notwithstanding policy 9.2.2.4, the following broad variety of residential, commercial, employment, institutional, civic, recreational and open space uses that will support a “fine-grained” urban street and a healthy pedestrian environment are permitted in the Mid-Rise Mixed-Use on Map 11.5.A:
- a. residential units;
  - b. small scale, grade related retail and service commercial, cafes and restaurant uses as part of a contiguous mixed-use building, with a maximum gross floor area of 200 square metres, where the street frontage width of any individual store front does not exceed a maximum of 10 metres;
  - c. small scale office uses that are a contiguous component of a mixed-use building, (located at grade);
  - d. public and institutional uses, public health facilities, places of worship, cultural and social facilities, (located at grade);
  - e. community facilities such as day care facilities, parks and public squares, (located at grade).
- 11.5.16.8. Notwithstanding policy 9.2.2.4 the following building types are permitted, pursuant to Section 9.2.3:
- a. Townhouses
  - b. Low-Rise buildings
  - c. Mid-Rise buildings
- 11.5.16.9. The street-facing ground level shall not include residential uses.
- 11.5.16.10. Stand alone, single use buildings are not permitted.
- 11.5.16.11. All buildings must be sited close to, and fronting, the street right-of-way.

### **11.5.17 Mid-Rise Residential**

- 11.5.17.1. The Mid-Rise Residential designation is intended to develop a residential neighbourhood that integrates residential and open space uses oriented to a scale that is pedestrian friendly.
- 11.5.17.2. The maximum building height within Mid-Rise Residential areas shall be a 13 metre (4 storeys) podium with 19 metres (6 storeys) maximum, stepping back on a 45 degree angular plane from the podium.

- 11.5.17.3. The minimum building height within Mid-Rise Residential areas shall be 8.5 meters (2 storeys).
- 11.5.17.4. The maximum lot coverage within Mid-Rise Residential areas shall be 80%.
- 11.5.17.5. The minimum setback Mid-Rise Residential areas shall be 3.0 meters
- 11.5.17.6. The maximum density within Mid-Rise Residential areas shall be 2.5 FSI.
- 11.5.17.7. Notwithstanding policy 9.2.2.3 permitted uses include:
  - a. residential units
  - b. parkettes and public squares;
  - c. one convenience retail store located at grade, with a maximum GFA of 100 square metres as part of a residential building.
- 11.5.17.8. Notwithstanding policy 9.2.2.3 the following building types are permitted, pursuant to policy 9.2.3:
  - a. Townhouses
  - b. Low-Rise buildings
  - c. Mid-Rise buildings
- 11.5.17.9. Notwithstanding policy 11.5.17.2, a maximum of 4 storeys are permitted facing the street. Additional storeys must step back at a 45 degree angle.

### **11.5.18 Private Open Space - The Fairgrounds**

- 11.5.18.1. The Fairgrounds is a unique area within Vaughan and requires a site specific designation. The intent of this Secondary Plan (and of the Woodbridge Heritage Conservation District) is to conserve the Fairgrounds as a significant cultural heritage landscape, and as a significant open space amenity within Vaughan.
- 11.5.18.2. The maximum building height within the Private Open Space/The Fairgrounds area shall be 11.0 meters (3 storeys).
- 11.5.18.3. Notwithstanding policy 9.2.2.17, permitted uses include:
  - a. outdoor markets;
  - b. a track for the racing of animals;

- c. any general farming, agricultural use or animal husbandry, which is not obnoxious to the public welfare, and such uses or enterprises as are customarily carried on in the field of general agriculture;
- d. office space associated with the management of the Fairgrounds;
- e. ancillary buildings associated with the Fairgrounds activities, may be considered and shall be designed to contribute to the framing and layout of the Fairgrounds;
- f. does not include a track for the racing of snowmobiles or any type of motorized vehicle, golf courses, golf driving ranges or miniature golf courses.

### 11.5.19 Parks

- 11.5.19.1. The Parks designation represents a variety of publicly accessible land uses that centre on an open space, ranging from a naturalized woodlot to a hard-landscaped public square. It is the intent of this Secondary Plan to develop a diverse range of open space types, connected through a larger network of open spaces.
- 11.5.19.2. Permitted uses include:
  - a. parks, playgrounds, sport and athletic fields, picnic areas, swimming pools, day camps, and outdoor passive amenities;
  - b. indoor and outdoor recreational facilities;
  - c. cultural uses;
  - d. public utilities, where required; and,
  - e. does not include a track for the racing of animals, snowmobiles or any type of motorized vehicle, golf courses, golf driving ranges or miniature golf courses.
- 11.5.19.3. Parks must be publicly accessible.
- 11.5.19.4. Parks must be easily accessible to pedestrians.
- 11.5.19.5. When a private property abuts an open space, buildings shall be designed to front the open space, with doors and windows, creating a built form edge to the public open space.
- 11.5.19.6. Blank walls, surface parking, garages, structured parking, and service areas should not front directly onto an open space / park.
- 11.5.19.7. Lands utilized for stormwater management facilities should not be considered as any part of the parkland dedication requirement provided through the Planning Act.

## 11.5.20 Regional Storm Flood Plain and Abutting Natural Areas

The Regional Storm Flood Plain and abutting Natural Areas (lands that extend from the flood plain to the TRCA Regulation Limit, whether in public or private ownership), are intended to conserve and enhance the natural ecosystem and to maintain biological diversity within Vaughan. Natural Areas have been identified by the TRCA and are regulated and managed by the TRCA and the City of Vaughan. The TRCA Regulation Limit is the area subject to the TRCA's Development, Interference with Wetlands and Alterations to Shorelines and Watercourses Regulation (Ontario Regulation 166/06). The Regulation Limit does not represent the development limit. The development limit is established through the application review process. Proposed development works for properties located within the Regulation Limit Area will require a permit under the Ontario Regulation 166/06.

### Regional Storm Flood Plain

- 11.5.20.1. Properties within the Kipling Avenue Study Area are, for the most part, above the flood line, meaning, these properties are located outside of the Regional Storm Flood Plain. Properties, or parts of a property that lie within the flood plain or Natural Areas as determined by the TRCA, must seek approval by the City of Vaughan and the TRCA for any development including:
- a. construction and/or reconstruction;
  - b. change of use to a building;
  - c. site grading;
  - d. temporary or permanent placement or removal of fill material; and,
  - e. the interference and alterations to watercourses, including the construction of storm water management ponds and outfalls.

### Natural Areas

- 11.5.20.2. Notwithstanding policy 9.2.2.16, permitted uses within the Natural Areas designation on Map 11.5.A include compatible recreational uses such as trails and low impact playgrounds.
- 11.5.20.3. No Conservation Lands shall be used in calculating the lot frontage, lot area, coverage or yards required for a development, or be considered as any part of the parkland
- 11.5.20.4. dedication requirement provided through the Planning Act.



- 11.5.20.5. A minimum 10 metre ecological buffer from the greater of the stable top of bank, dripline of significant vegetation or Regional Storm Flood Plain shall be provided to the City of
- 11.5.20.6. Vaughan, outside of the development lot or block, adjoining a valley or stream corridor, as part of the public open space system.

### 11.5.21 Urban Design - Built Form

The built form framework defines the desired future character and function of built elements within the Kipling Avenue Corridor Area. The intent is to ensure that new buildings reinforce a coherent, harmonious and appealing urban environment, as well as contribute to the enhancement of the public realm.

#### Height Zones

- 11.5.21.1. Height, in this Secondary Plan, is determined by the land use designations as follows:
  - a. **Low-Rise Mixed-Use** – 8.5 m (2 storeys) minimum; 11 m (3 storeys) maximum
  - b. **Low-Rise Residential A** – 4 m (1 storey) minimum; 8.5 m (2 storeys) maximum
  - c. **Low-Rise Residential B** – 8.5 m (2 storeys) minimum; 11 m (3 storeys) maximum
  - d. **Low-Rise Residential C** – 8.5 m (2 storeys) minimum; 13 m (4 storeys) maximum
  - e. **Mid-Rise Mixed-Use** – 8.5 m (2 storeys) minimum; 19 m (6 storeys) maximum
  - f. **Mid-Rise Residential** – 8.5 m (2 storeys) minimum; 19 m (6 storeys) maximum
  - g. **Private Open Spaces** – The Fairgrounds – 11 m (3 storeys) maximum
  - h. **Parks** – n/a
  - i. **Natural Areas** – n/a
- 11.5.21.2. To ensure that the building envelope and height make a positive contribution to adjacent properties and the public realm, the following must be considered with all new or renovated buildings:
  - a. Consider the human scale:
    - i. The “human scale” makes reference to the experience of the building mass in relation to the size of its users. Buildings and the elements which constitute the façade must have a proportion and scale that is welcoming to

its users. For example, the relative size of a door, a window, or a staircase should be proportioned with the scale of users. The human scale is most important in areas that are accessible or visible from the public realm, such as the first few storeys of a building facing the street.

- b. Encourage a fine grain:
  - i. A “fine grain” makes reference to a pattern of buildings, or design elements within a building, that are small in scale and change frequently, thereby creating a dynamic and animated environment for pedestrians.
  - ii. Buildings should be visually divided into a hierarchy of building volumes, facades that introduce a pattern of doors and windows, and a ground level that offers a variety of experiences in a short interval. Avoid large blank walls or an uninterrupted building mass.
- c. Consider sunlight and shadows:
  - i. Light and shade are one of the most measurable ways in which a building mass will have an impact on its surroundings. New buildings should be able to maximize the development potential, without causing undue shadow impacts on adjacent properties or public spaces.
  - ii. Protecting adequate sunlight penetration for residential uses and public spaces, including streets, parks, open spaces and other recreational areas, will be most important during the spring and fall seasons.
- d. Provide a continuous podium:
  - i. In areas such as the Woodbridge Avenue and Kipling Avenue commercial node, pedestrians will interact closely with the first 2 to 4 storeys of a building. At this level it is important to provide a continuous street wall. The continuity of the building base along the full length of the property and street with animated uses is a good way to provide a well-defined and welcoming pedestrian environment.
  - ii. Additional height, above the podium base, should step-back. Terracing higher portions of the building will protect views, sunlight penetration, and the skyview.
- e. Provide a transition in heights:
  - i. Notwithstanding the allowable height, buildings should respect the scale of adjacent buildings by providing a proper transition in heights. Designing a proper transition is most important when new development is adjacent to: heritage properties, a lower density land use, or a public open space.

### **Building frontages, orientation and placement**

- 11.5.21.3. Building frontages, orientation and placement are shown on Map 11.5.I.
- 11.5.21.4. The siting of buildings, in terms of their location within a property and in terms of the orientation of their facades, are a critical factor in the definition of a welcoming and safe pedestrian environment along streets and public spaces.
- 11.5.21.5. All building frontages must be oriented toward public streets and other public spaces, in order to clearly define the public realm, create a consistent street wall, and to create an attractive street environment for pedestrians.
- 11.5.21.6. All buildings within the study area, oriented toward public streets, must have clearly defined primary entry points that open directly on to the public sidewalk.
- 11.5.21.7. For all new development, buildings must be sited to first address the intersection or corner locations, secondly the primary street frontages, and thirdly the local street frontages and other publicly accessible open spaces.
- 11.5.21.8. Architectural features are encouraged at all corner building locations to enhance the visual prominence and identity of the Corridor.
- 11.5.21.9. Buildings located along the river valley and/or any other public space must have double frontages; facing both the street and the river valley and/or any other public space, but will only require primary entry points from the street.
- 11.5.21.10. Pavilion style buildings within a public square must have frontages on all sides and service entrances for these buildings should be kept to a minimum and be discretely integrated in the design of the building facades.
- 11.5.21.11. For all new development, parking will not be permitted between the edge of the public right-of-way or public realm and the building face or setback area.

### **Minimum Building Front-yard Setbacks**

- 11.5.21.12. To achieve a continuous street wall for all streets within the area, and especially along Kipling Avenue, a set of building setback conditions must be applied for all new development, as illustrated in Map 11.5.J. Generally, a minimum 3 metre building setback should be applied along most streets within the Corridor except at commercial nodes, where building restrictions exist (such as at the existing northern zero setback condition of the heritage church at Porter Avenue West), and along all neighbourhood streets within the Low-Rise Residential fabric. The building setback conditions identified on Map 11.5.J apply for all new development except where heritage contributing buildings on either side of the subject site are set further back from the front property line; then the setback will be the average of the front yard setbacks of the two properties on either side.

- a. With the exception of some parts of Kipling Avenue, Woodbridge Avenue and intersection, and a few of the existing neighbourhood streets, the minimum building setback should be 3.0 metres from the right-of-way.
- b. Along Woodbridge Avenue, and at the Woodbridge/Kipling Avenue gateway, there should be a zero setback condition to the right-of-way. The City of Vaughan may require additional setbacks in conditions where streetscape and public realm improvements are needed to create an improved pedestrian environment.
- c. Along the special streets such as Porter Avenue East and West, with the exception of the existing heritage church pinch point on the southern side of Porter Avenue West, there should be a minimum 4.5 metre setback from the right-of-way to accommodate enhanced landscaping, special streetscape features and green connections to major open spaces within the Corridor.
- d. All new main roadways such as Parkside Drive and Rainbow Creek Road should have a minimum 3.0 metre building setback from the right-of-way.
- e. All other residential streets should have a minimum 4.5 metre building setback from the right-of-way.
- f. Buildings adjacent to the valley lands must meet the setback requirements of the TRCA Regulation Limit.

## **Views**

- 11.5.21.13. As Kipling Avenue runs along a topographic ridge, significant views open towards the river valleys east and west. These views are a defining characteristic of the Corridor, and provide a significant opportunity to establish a quality urban experience. These views, as identified in Map 11.5.K, must be protected and enhanced.
- a. Existing views to the river valleys should be maintained and enhanced.
  - b. New views to the valley corridors should be established where ever possible.
  - c. Where possible, view opportunities should be associated with, and be accessible along, the trail system.
  - d. Views to open space and built form landmarks should be maintained and enhanced.

## **Ground Level Use, Entrances**

- 11.5.21.14. The design, use and animation of the ground level of buildings are significant elements in defining the character and experience of the street as a welcoming and safe pedestrian environment. Ensuring that buildings provide an attractive and animated face, especially at the ground level, is a priority.

- a. All mixed-use buildings, or buildings with a commercial use must have the commercial use located at grade and have identifiable at grade entrances fronting the street.
- b. All commercial use building entrances must be flush with the public sidewalk.
- c. All corner buildings should have corner entrances where possible.
- d. All ground levels uses, including residential uses, should have a prominent presence on the street.
- e. All residential units located at grade must have a direct entrance from the street that functions as a front door and has the characteristics of a front door.

### **Corner and terminus sites**

11.5.21.15. Corner and terminus sites are shown on Map 11.5.I.

11.5.21.16. Some sites, buildings and/or portions of buildings, given their visual prominence, have a special role to play within the urban design of the street.

- a. To enhance the distinction and landmark quality of new buildings on corner or visual terminus sites, modest exceptions to special features of buildings are permitted to setbacks of 3 metre maximum and height to 3 metre maximum restrictions may be permitted to encourage massing and designs that accentuate the visual prominence of the site – architectural treatments can include tall slender elements such as spires or turrets that are complimentary to the surrounding heritage character.
- b. New developments on corner sites should orient to both street frontages.
- c. New developments on terminus sites should align design features to the view axis which, in addition to tall elements, can include aligned entries or portico openings.
- d. As new development on corner and terminus sites can shape the image and character of an area, they should have greater civic obligations to ensure that the highest possible standards in design and material quality are achieved.

### **Parking and Servicing**

11.5.21.17. Parking and servicing are a necessary aspect of any development. They should be fully integrated within buildings, and directly linked to the areas where they are most effective. In general, parking and servicing should be located within the development block, and not adjacent to, and or visible from, the street or pedestrian areas.

- a. Wherever possible, parking for new developments should be provided below-grade and accessed by a rear or side yard lane.
- b. Surface parking and service areas should be located to the rear of a development and generally not visible from the street.
- c. Where surface parking or service areas are exposed, they should be designed to include landscaping, clearly marked pedestrian access and paths, lighting and buffered with landscaping such as hedges or shrub planting and/or other mitigating design measures.
- d. Where a structured public parking facility is visible, it should be designed to be integrated into the surrounding context by using similar façade articulation and materials, including small openings that function as ‘windows’.
- e. Where a structured parking facility fronts onto a street or public space, the building frontage should be faced with active uses.

### **Landscaping and Amenity**

11.5.21.18. For new development, especially in areas where there are higher building heights and densities, private open space enhancements are encouraged as part of the built form design in order to contribute to the visual aesthetics and quality of the public realm.

- a. On broad sidewalks in retail areas, outdoor spill-out activities such as patios are encouraged to further animate the street.
- b. Roof gardens should be encouraged on all above-grade surfaces especially on structured parking roof tops.
- c. Balconies should be designed as an integral part of the building rather than appearing to be “tacked-on”.
- d. Courtyards, forecourts and other intimate spaces accessible to the public and animated with at-grade uses are encouraged.

### **11.5.22 Heritage Conservation**

A significant part of the Kipling Avenue Corridor area lies within the Woodbridge Heritage Conservation District (Woodbridge HCD Study Area). The Woodbridge HCD Study, to date identifies that the heritage character of certain areas within the Woodbridge Core, Kipling Avenue and some adjacent streets should be conserved. To do so, it describes the heritage character and prescribes guidelines to manage change within a defined district boundary.

11.5.22.1. Heritage conservation is a core principle of this Secondary Plan. The Plan considers that heritage conservation of heritage properties (See Map 11.5.L), structures and

landscapes, is key to achieving an attractive and liveable urban environment that will be cherished by residents and visitors alike. To this end:

- a. Heritage resources and/or properties within the boundary of the Woodbridge HCD must abide by the policies and guidelines of the Woodbridge HCD Plan. Where conflict may arise between both documents, the Woodbridge HCD Plan will take precedence over this Secondary Plan.
- b. Properties beyond the boundary of the Woodbridge HCD, must be sympathetic to the heritage character and attributes described in the Woodbridge HCD Plan and provide an appropriate transition to the Woodbridge HCD.
- c. Properties that contain heritage resources that have been listed or designated as part of the City of Vaughan Heritage Inventory, must conserve the heritage character and heritage attributes identified as part of the listing and/or in the Designation Report.
- d. Properties that are adjacent or near heritage resources that have been listed or designated as part of the City of Vaughan Heritage Inventory, must be sympathetic to the heritage character and attributes identified as part of the listing and/or in the Designation Report.
- e. Notwithstanding the above, the City of Vaughan may require a Heritage Impact Assessment as part of the application process for any municipal approval including new construction, renovation or demolition of a structure or landscape. The City of Vaughan may also require a Conservation Plan secured by a letter of credit, as part of any municipal approval, for properties where heritage resources have been identified either as part of the City of Vaughan Heritage Inventory, as part of the Woodbridge HCD, or as a result of the Heritage Impact Assessment.

### **11.5.23 Transitions of New Buildings in Relation to Heritage Resource**

- 11.5.23.1. One of the key driving factors for this Secondary Plan is the conservation of the existing heritage fabric within the Kipling Corridor which includes structures and landscapes and open space that contribute to the existing heritage character. The intent is to establish a framework for development that guides the integration of new structures within the existing heritage context, mainly along Kipling Avenue.
- 11.5.23.2. The Kipling Avenue Corridor Study supports the recommendation of the Woodbridge HCD Study in relation to the existing contributing heritage fabric which states that, “new and renovated structures and landscapes, as well as additions to existing structures and landscapes, must be sympathetic to the heritage character and the heritage attributes of ‘adjacent’ heritage resources. The term ‘adjacent’ includes properties that a) touch; b); form part of a cluster; c) form part of a continuous street wall; and d) are visible from each other.
- 11.5.23.3. Once approved, the Woodbridge HCD will be the governing policy over this study and will be used as the mechanism to conserve the structures and landscapes that

contribute to the HCD's heritage character, and to manage the introduction of new structures and landscapes in such a way that they harmonize with heritage resources and contribute to the district's heritage character.

### **The HCD Study Guidelines**

- 11.5.23.4. The following guidelines, as established in the Woodbridge HCD Study, shall be used to assist in the process of achieving the proper transition of building scales, heights and presence in order to create a harmonious relationship between new structures and landscapes with contributing properties that fall within heritage resources within the Kipling Avenue Corridor Area.
- 11.5.23.5. The term “contributing buildings” describes the buildings that have been identified through the Woodbridge HCD Study as contributing to the heritage character of the Woodbridge HCD, and which cultural heritage value must be conserved. A list of the properties with contributing buildings is included on Map 11.5.L.
- a. Conservation of Heritage Resources
    - i. Buildings and structures listed as ‘contributing’ in the Woodbridge HCD may not be demolished or moved.
    - ii. The City of Vaughan may require a Heritage Impact Assessment when it considers that cultural heritage value may exist, or be effected by any new construction.
  - b. Conservation of Heritage Character
    - i. Contributing buildings display a variety of setbacks and side yard conditions, reflecting the different construction periods and original use.
    - ii. New development must be sympathetic to this character and must develop in a way that does not detract, hide from view, or impose in a negative way, on existing heritage contributing resources, as per the following height and setback guidelines.
  - c. The historic setbacks of contributing buildings should be maintained and contributing buildings should not be relocated to a new setback line. New buildings must be sympathetic to the setbacks of adjacent contributing buildings.
  - d. Height Guidelines
    - i. The height of contributing buildings should be maintained.
    - ii. New buildings must be sympathetic to, and transition from, the height of adjacent contributing buildings with a minimum 45 degree angular plane, starting from the existing height of the contributing building, measured at the building's edge.



e. Sideyard and Backyard Setback Guidelines

- i. New buildings must setback from contributing buildings a distance equivalent to half the height of the contributing building.
- ii. Consideration may be given to the construction of new buildings, and additions to contributing buildings, adjacent to, or joining with, contributing buildings only when:
  - A. new construction is located in the parts of the contributing building that is not visible from the street or from a public space;
  - B. new construction is setback from the street frontage of the contributing building, maintain the heritage character of the street;
  - C. the parts of the contributing building that will be enclosed or hidden from view by the new construction, do not contain significant heritage attributes; and,
  - D. new construction contributes to the district's heritage character.

f. Frontyard Setback Guidelines

- i. The historic setbacks of contributing buildings should be maintained and contributing buildings should not be relocated to a new setback line. New buildings must be sympathetic to the setbacks of adjacent contributing buildings.
  - ii. When new buildings are located adjacent to existing contributing buildings that are set back from the property or street line, new buildings should transition back to the setback line of existing contributing buildings in order to maintain open views and vantage points from the street to the contributing buildings.
- g. Along Kipling Avenue, where heritage contributing buildings are set further back from the recommended 3.0 metre minimum building setback line, any new development adjacent to the heritage contributing building must be set back, at a minimum, to a line measured at 45 degrees from the front corner of the existing heritage contributing building, (see Appendix B, Diagram E).

h. Landscape Guidelines

- i. New buildings and landscapes must contribute to the heritage character and attributes of adjacent contributing landscapes such as parks, public squares, open spaces, recreational areas, and landscaped areas within private properties.
- ii. Pedestrian connections between adjacent landscaped areas should be maintained and enhanced.

- iii. Significant visual connections between adjacent landscaped areas should be maintained and enhanced.
- iv. New and renovated buildings must provide an active, welcoming façade facing significant landscapes and public spaces.
- i. Architectural Character Guidelines
  - i. New Buildings-Architectural Design
    - A. Within the heritage district new architecture will invariably be constructed. This will occur as buildings on vacant sites, replacement buildings for non-contributing existing structures or severely deteriorated older buildings.
    - B. New buildings will fall into two categories, replica or reconstructed buildings and contemporary buildings.
  - ii. Replica/reconstructed buildings
    - A. On some sites the opportunity may exist to replicate a formerly existing structure with a new building or as a part of a larger building proposal. This approach is to be encouraged where good documentary evidence exists. The replication of a historic building should proceed in a similar manner to the restoration of an existing but altered or deteriorated structure.
    - B. Design of the building should be based upon documentary evidence including photographs, maps, surveys and historic design/construction drawings.
    - C. Replica designs should be prepared by practitioners skilled in historic research and historic construction design and detailing.
    - D. The interior space and basic structure of a replica building is not required to, but may also, use historic materials or details as long as the exterior presentation replicates the original structure.
  - iii. New buildings in the heritage district
    - A. Entirely new buildings may be proposed where no previous buildings existed, or where original buildings are missing or severely deteriorated, and where noncontributing buildings have been removed.
    - B. The intention in creating designs for new buildings should not be to create a false or fake historic building, instead the objective must be to create a sensitive well designed new structure “of its time” that fits and is compatible with the character of the district and its immediate context. Designers of new buildings in the district should have a proven track record respecting the creation of designs in similar historic contexts.

- C. The design of new buildings in the HCD should carefully consider requirements elsewhere in this document for density, scale, height, setbacks, coverage, landscape open space, view corridors, angular plane and shadowing. Further, character areas have been identified in the district. Each character area has identifiable characteristics including commercial mainstreet as opposed to residential, building scale spacing and setback, which should also be understood and respected.
  - D. Architectural considerations include the following attributes: contemporary design, material pallet, proportions of parts, solidity verses transparency and detailing.
- iv. Contemporary design
    - A. Contemporary work should be “of its time”. This is consistent with the principals stated in the Venice Charter, Appleton Charter and other Charters recognized internationally as a guide for heritage work. This does not mean that new work should be aggressively idiosyncratic but that it should be neighbourly and calmly, respectfully, fit its “village” context while at the same time representing current design philosophy. Quoting the past can be appropriate, however, it should avoid blurring the line between real historic “artifacts”, buildings, bridges and other structures.
    - B. Contemporary as a design statement does not simply mean current. Current designs with borrowed detailing inappropriately, inconsistently, or incorrectly used, such as pseudo- Victorian detailing, should be avoided.
  - v. Material pallet
    - A. Whereas there is a very broad range of materials in today’s design pallet, materials proposed for new buildings in the district should include those drawn from ones historically in use in Woodbridge. This includes brick, stone, stucco, wood siding and trim; glass windows and storefronts and various metals. The use and placement of these materials in a contemporary composition and their incorporation with other modern materials is critical to the success of the fit of the proposed building in its context. The proportional use of materials, drawing lines out of the surrounding context, and careful consideration of colour and texture, all add to the success of a composition.
  - vi. Proportions of parts
    - A. Architectural composition has always had at its root the study of proportion. In various styles rules of proportion have varied from the complex formulas of the classical orders to a more liberal study of key proportions in buildings of the modern movement.

- B. In the design of new buildings in this heritage district, work should take into account the proportions of buildings in the immediate context and consider a design with proportional relationships that make a good fit.
  - C. An example of this might be windows. Nineteenth century buildings without fail used a vertical proportion system in the design and layout of windows, including both overall windows, singly or in built up groups, and the layout of individual panes.
  - D. In buildings of the art deco and art modern period, windows are often found to be of a horizontal proportion. Although not universally the case, this horizontal proportioning is a character defining feature of these styles.
- j. Solidity versus transparency
- A. Similar to proportion, it is a characteristic of historic buildings of the 19th century to have more solid walls with punched windows. This relationship of solid to void makes these buildings less transparent. It was a characteristic that was based upon technology (ability to make large windows and to heat space), societal standards for privacy, and architectural tradition. Buildings of many 20th century styles in contrast use large areas of glass and transparency as part of the design philosophy.
  - B. In this historic district the relationship of solidity to transparency, is a characteristic of new buildings that should be carefully considered. It is an element of fit. The nature of the immediate context for the new building in each of the defined character areas should be studied. The level of transparency in the new work should be set at a level that provides a good fit on the street frontages.
  - C. In the Woodbridge Avenue character area, a main street approach can be taken and a more transparent building permitted between the proportion of 20% solid to 70 % solid.
  - D. In the other character areas this proportion should reflect a more traditional residential proportion of 40% solid to 80% solid.
- i. Detailing
- A. In contemporary design philosophy the expression goes “God is in the details”, this is to say a beautifully detailed building is usually a successful one (alas not always so). In part this is due to the expressive nature of the fundamental construction components of modern buildings.
  - B. In past styles, for example, structure was often hidden behind a veneer of other surfaces and “detailing” was largely provided by the use of coloured, shaped, patterned or carved masonry and /or added traditional ornament, moldings, finials, cresting and so on. In contemporary

buildings every element of a building, such as architectural, structural, mechanical even electrical systems, can potentially add to the artistic composition.

- C. For new buildings, the detailing of the work should once more refer to the nature of the immediate context and the attributes of the area in which it is to be placed.

### 11.5.24 Parks and Open Space

11.5.24.1. The following Parks policies apply to the Secondary Plan Area:

- a. The City may acquire lands anywhere within the Secondary Plan for the purposes of establishing parkland. The parkland designated by this Plan shall be shown on draft plans of subdivision. Their actual size, configuration and design will be subject to confirmation by the City during the development approval process.
- b. All development applications may be required to identify a potential location for an urban square, to the satisfaction of the City.
- c. Where the City has determined that parkland dedication is not required on an individual development site, cash-in-lieu of parkland dedication shall be payable to the City in accordance with the Planning Act and the City's cash-in-lieu policy.
- d. All parkland design and construction shall be carried out by the City in accordance with the City's Development Charges By-law. However, in the event that the City and the relevant landowner agree that the owner will develop the park, the design, securities and construction for the park will be addressed through an agreement in accordance with the City's "developer-build/parks development policy".
- e. Public squares are intended as formal spaces for passive recreation, in support of the adjacent development. Urban squares should address the following design guidelines:
  - i. Public squares are to be planned as focal points;
  - ii. Relationships with adjacent buildings should provide optimal sunlight penetration, sky view and wind conditions;
  - iii. Streetscape and buildings that abut a public square should be designed to reinforce an interactive relationship between the open space and its adjacent land use;
  - iv. Buildings should front onto a public square to create built form edges to the public space;

- v. The landscape along the street frontage, including high canopy street trees, should be complementary on both sides of the street;
- vi. Entry and access points should be located conveniently and incorporate civic design themes; and
- vii. Hard and soft landscape elements and features shall be designed to define and articulate activity areas, circulation, entry points, seating and gathering areas.

### **11.5.25 Environmental Policies**

- 11.5.25.1. Redevelopment of lands for residential purposes on lands currently occupied by industrial uses, and abutting or adjacent to industrial uses or CPR lands, shall require that the City and the Ministry of the Environment and Energy be satisfied that there are no environmental or land use impacts. A Phase One and Two Environmental Report shall be submitted.
- 11.5.25.2. Development shall occur in a manner consistent with the City's philosophy of ecosystem planning which provides for the protection and conservation of the quality of environmental resources. The objectives associated with this principle are:
  - a. To provide for a system of storm water management for both water quality and quantity, through the application of state-of-the-art Stormwater Management Practices, designed to protect lands and receiving waters within and outside the Amendment Area from detrimental effects such as erosion, flooding and sedimentation resulting from increased levels and rates of runoff.
  - b. To ensure that environmental impacts are addressed in an ecologically responsible manner, consistent with the City's environmental policies.
  - c. To encourage environmental remediation measures within new developments, where appropriate.
  - d. To encourage the use of passive and active renewable energy sources, where appropriate.
  - e. To encourage the use of 'green' technologies in the design of new buildings and infrastructure, where appropriate by providing easy access to public transit.
  - f. To reduce vehicular dependency by locating residential and work related uses in proximity to one another in order to encourage residents to both live and work in vicinity, and by providing easy access to public transit.

### **11.5.26 Stormwater Management Ponds**

- 11.5.26.1. Stormwater management facilities shall be permitted in all land use designations.

## 11.5.27 Transportation

- 11.5.27.1. Currently the entire Kipling Avenue Corridor is operating at acceptable levels of service.
- 11.5.27.2. The entire Corridor will approach its roadway capacity with the proposed development scenario. The Plan encourages a shift in environment that caters to cyclists and pedestrians, slower speeds to calm traffic, and an increase in the orientation to transit.
- 11.5.27.3. There are opportunities to revise the Kipling Avenue Street cross section north of Woodbridge to include single-sided on-street parking and the addition of two bike lanes.
- 11.5.27.4. Much of the vehicular traffic congestion in the area currently is due to background traffic (vehicular trips through the area, rather than from the area).
- 11.5.27.5. As the proposed development scenario is built out, the increased local traffic will have the effect of reducing through traffic from other areas.
- 11.5.27.6. There are opportunities to redesign Kipling Avenue south of Woodbridge Avenue to have two travel lanes, two parking lanes, bike lanes and no left turning lanes.
- 11.5.27.7. Prior to any development or redevelopment within the Secondary Plan Area, applicants will prepare a detailed traffic impact study to the satisfaction of the City and the Region, which will address in part, but not limited to capacity, access, function, neighbourhood infiltration, transportation and transit objectives.
- 11.5.27.8. Streetscape design for arterial roads, collector roads, and local roads shall be subject to the urban design requirements and policies articulated in this Secondary Plan.
- 11.5.27.9. In terms of streetscape design, the main objectives of the Streetscape Master Plan with respect to the Transportation System are as follows:
  - a. Walkable, connected pedestrian oriented streets.
  - b. A reduction of road widths providing ample opportunity for enhanced landscaping and generous sidewalks.
  - c. Reduced front car parking garage access onto Kipling and the development of rear lane access where possible.
  - d. Defined street edges with frontages to the street.
  - e. An elimination of surface parking fronting public streets.

- f. Publicly accessible streets, discouraging private roads and laneways.
- g. A cohesive character and identity for each street typology.
- h. On-street parking and bike lanes where possible.
- i. Safe street and rail crossings.
- j. An identification of opportunities for enhanced streetscape treatments and pedestrian priority zones.
- k. Increased transit service and streetscaping that supports increased transit use.

### **Street Network**

11.5.27.10. The Street Network for the Kipling Avenue Corridor area is shown in Map 11.5.D. Notwithstanding which, any additional road, public or private, that is deemed by the City to be necessary to the efficient implementation of this Street Network can be constructed without further amendment of this Secondary plan.

11.5.27.11. Reduced roadway and transit standards should be encouraged in order to minimize street widening and increase the opportunity for a generous public realm.

11.5.27.12. The Street Network road widths shall be:

- a. Travel lanes - 3.0m - 3.3m wide
- b. Combined Travel and Transit lanes - 3.75m wide
- c. Transit lanes - 3.75m wide
- d. On-street parking lanes - 2.5m wide
- e. Bike lanes - 1.45 - 1.5m

### **Street Connections and Linkages**

11.5.27.13. The vision strives to achieve a pattern of streets and blocks that enable the present and future development of complete and accessible neighbourhoods for the corridor. As stated in Section 11.5.3: Block Pattern, Street Network and Linkages, achieving a permeable block system and a fine network of vehicular and pedestrian connections is desirable. As well, establishing new access points and linkages to main streets, parks, public amenities and new residential development is an integral component in the development of the Corridor and in shifting the existing vehicular oriented transportation system to a pedestrian oriented, multi-modal system.



- 11.5.27.14. Additional study and site review should be undertaken to ensure that the values inherent in the Transportation Framework are achieved and are in keeping with the long term vision for the Corridor.

### **Public Transit**

- 11.5.27.15. The street cross -section for Kipling Avenue allows for the continued accommodation of a southbound transit loop as well as the opportunity for increased service. As the Corridor develops, a full time transit service along Kipling Avenue is recommended especially as the new Rainbow Creek neighbourhood develops.
- 11.5.27.16. As well, improved transit facilities should be provided such as increased transit stops, bus shelters and signage. Transit furnishings should be complimentary to the Streetscape Master Plan design and should be considered holistically along with other street furniture, prior to implementation of the master plan. Transit furnishings should also be complementary to the heritage character and should be reviewed by the City of Vaughan Cultural Services Staff and Planning Department Staff.

### **Parking**

- 11.5.27.17. All streets should be considered for on-street parking, especially along Parkside Drive to ensure public access to the river valley trails and parks.
- 11.5.27.18. On- street parking should be accommodated along the main streets such as Kipling and Woodbridge Avenue as it provides a safety buffer for pedestrian movement and increases the opportunity to establish a healthy commercial presence.
- 11.5.27.19. Wherever possible, and as redevelopment occurs, parking garages and front parking access should not be allowed along Kipling Avenue with the exception of the existing heritage buildings that already have this condition. Parking access and garages should be located at the rear of buildings.
- 11.5.27.20. Parking and servicing access to buildings should not be allowed to front Kipling Avenue.
- 11.5.27.21. Landscaping, fencing, architectural elements and other appropriate screening treatments should be provided to reduce the impact of at grade parking (where existing) and service areas, and contribute to the visual continuity of the public realm, ensuring that safety and security measures are maintained.
- 11.5.27.22. Wherever possible, the City shall encourage cash-in-lieu of parking, pursuant to the provisions of the Planning Act and Municipal Act.

### **11.5.28 Phasing of Development**

- 11.5.28.1. Phasing of development may be addressed through the appropriate use of the holding (H) provisions of this plan.
- 11.5.28.2. Development Applications within the new Rainbow Creek residential area shall provide a Phasing Plan which:
  - a. Describes and illustrates how existing and proposed development can be incorporated into the site to achieve the full development potential of the site.
  - b. Identifies the public infrastructures and facilities required to serve the development, including water, sewer, stormwater, roads, transit, parks and open space facilities, and their proposed phased construction.
  - c. Describes the expected financial requirements for such public infrastructure, and the appropriate financial contributions from benefitting landowners.
  - d. Given that the Rainbow Creek area will be undergoing a conversion of land use from the existing industrial uses to residential uses, and that this conversion will occur in different phases as landowners decide to re-develop their particular lands; it is therefore understood that re-development of individual parcels may be dependent on the land use status and zoning of abutting lands. Any development application within the Rainbow Creek industrial lands should adhere to the Ministry of the Environment (MOE) Guidelines pertaining to land use separation distance requirements. The approval of any development applications within the Rainbow Creek industrial lands is conditional upon achieving land use separation distances that are satisfactory to the City of Vaughan.
  - e. It should be noted that the MOE Separation Distance Guidelines apply throughout the Secondary Plan Area.

### **11.5.29 Developers' Group Agreement**

- 11.5.29.1. Prior to final approval of any draft plans of subdivision/draft plan of condominium/site development plan for lands location within the new Rainbow Creek neighbourhood, the landowner shall enter into a developer's group agreement with landowners who fall within their phase of development (Either Phase 1 or Phase 2) to the satisfaction of the City. The agreement shall be regarding, but not limited to, all cost sharing for the particular phase of development, for the provision of parks, cash-in-lieu of parkland, roads and municipal services within that development area. This agreement shall include a provision for additional developers to participate in the Developers' Group Agreement when they wish to develop their lands.
- 11.5.29.2. Prior to final approval of any draft plan of subdivision/draft plan of condominium/site development plan application, the Trustee for each of the phases of development shall provide the City with a letter indicating that the Owner has fulfilled all cost

sharing and other obligations of the Rainbow Creek Phase 1 or 2 cost sharing and other obligations of the Developer Agreement.

### 11.5.30 Interpretation

- 11.5.30.1. This Secondary Plan is a statement of policy. It is intended to guide the development of the Amendment Area. Some flexibility in interpretation is permitted, provided the intent of the policies and principles of this Plan are maintained.
- 11.5.30.2. The designations identified on Map 11.5.A are intended to show general areas. Minor adjustments to the boundaries of the designations may occur through the City's approval process without amendment to the Plan, except where the designations are established by fixed boundaries, such as road rights-of-way, or where specifically stated to be fixed in the policies of this Secondary Plan.
- 11.5.30.3. Where lists or examples of permitted uses are provided, they are intended to indicate the possible range and type of uses that are to be considered. Specific uses not listed, but considered by the City to be similar to the listed uses and to conform to the general intent of the applicable land use designation may be recognized as a permitted use and recognized in the implementing zoning by-law.
- 11.5.30.4. Minor variations from numerical requirements in the Plan may be permitted without an Official Plan Amendment provided that the general intent of the Plan is maintained. Minor variations, in this Secondary Plan, shall mean a maximum of 5 percent variation from the identified numerical requirement.

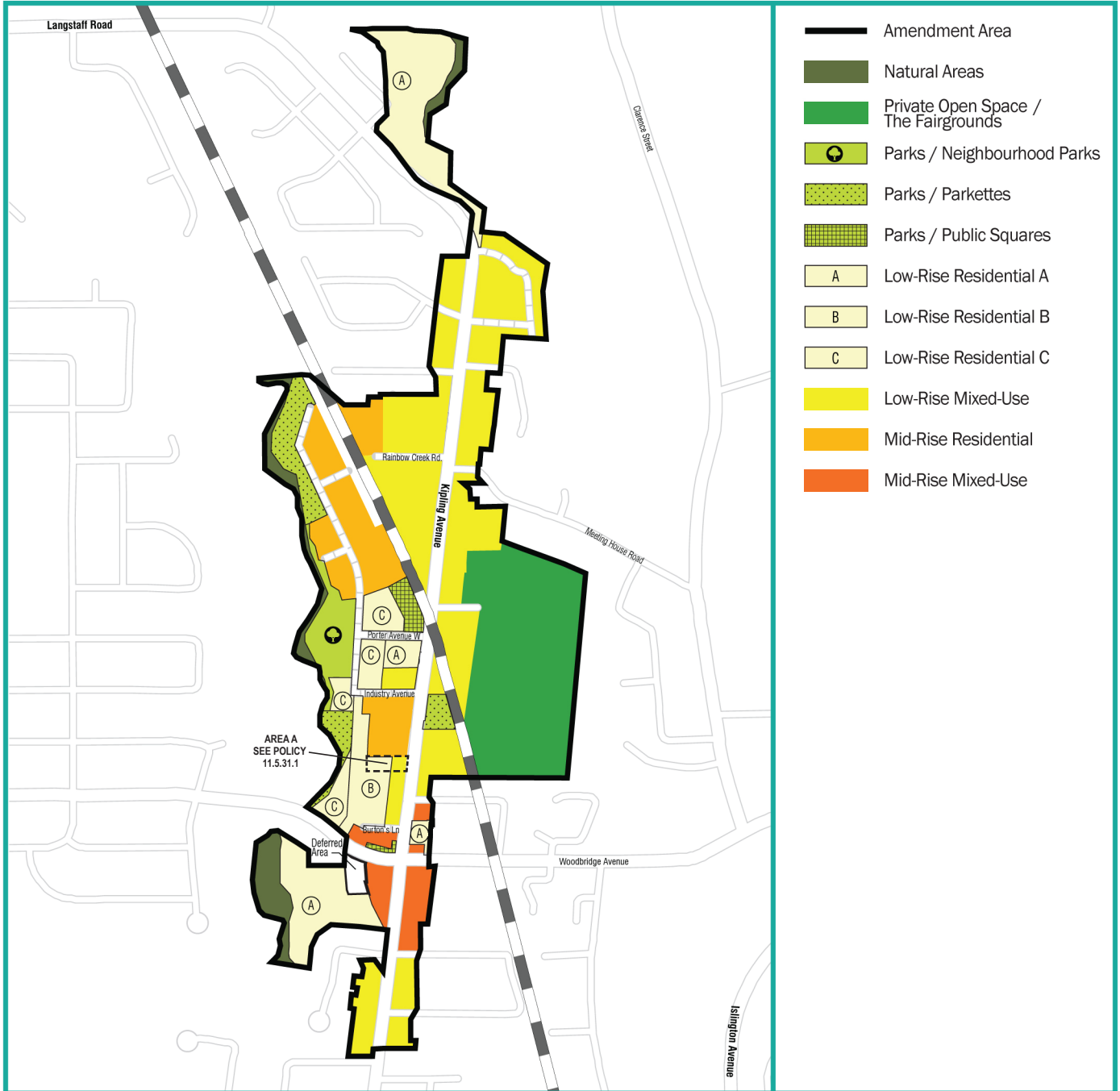
#### 11.5.31 Site Specific Policies













(OPA #44)

- 11.5.31.1 The following shall apply to the lands municipally known as 8010 Kipling Avenue, identified as "Area A – Policy 11.5.31.1":
  - a) Notwithstanding Policies 11.5.2.6, 11.5.12 and 11.5.14 the following shall apply:
    - i. Permitted use shall be only a 3-storey, Low-Rise Building;
    - ii. The maximum lot coverage shall be 70%;
    - iii. The minimum setback to Kipling Avenue shall be 2.5 m;
    - iv. The maximum density shall be 1.94 FSI;
    - v. The maximum number of residential units shall be 45.
  - b) As conditions of Site Plan approval, the Owner shall:
    - i. Provide an easement for the purposes of a pedestrian walkway connection, should it be required in the future, to the satisfaction of the City;
    - ii. Register a restrictive covenant on the title of the property requiring that the building be operated as a private market purpose built residential rental apartment building for a minimum of 20 years.



▼ Map 11.5.A  
Kipling Avenue - Land Use

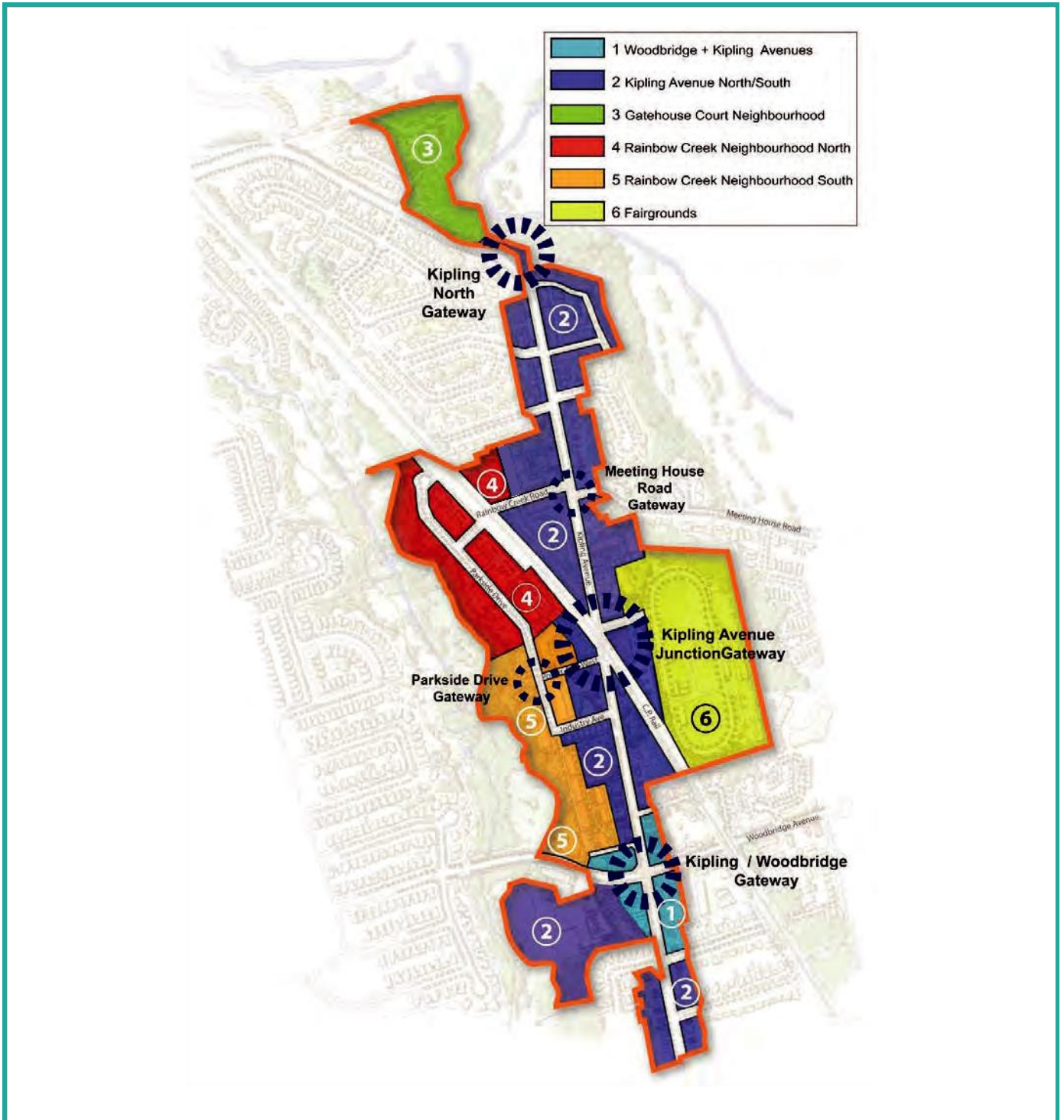


-  Amendment Area
-  Natural Areas
-  Private Open Space / The Fairgrounds
-  Parks / Neighbourhood Parks
-  Parks / Parkettes
-  Parks / Public Squares
-  Low-Rise Residential A
-  Low-Rise Residential B
-  Low-Rise Residential C
-  Low-Rise Mixed-Use
-  Mid-Rise Residential
-  Mid-Rise Mixed-Use

0 250 500 1,000  
Meters

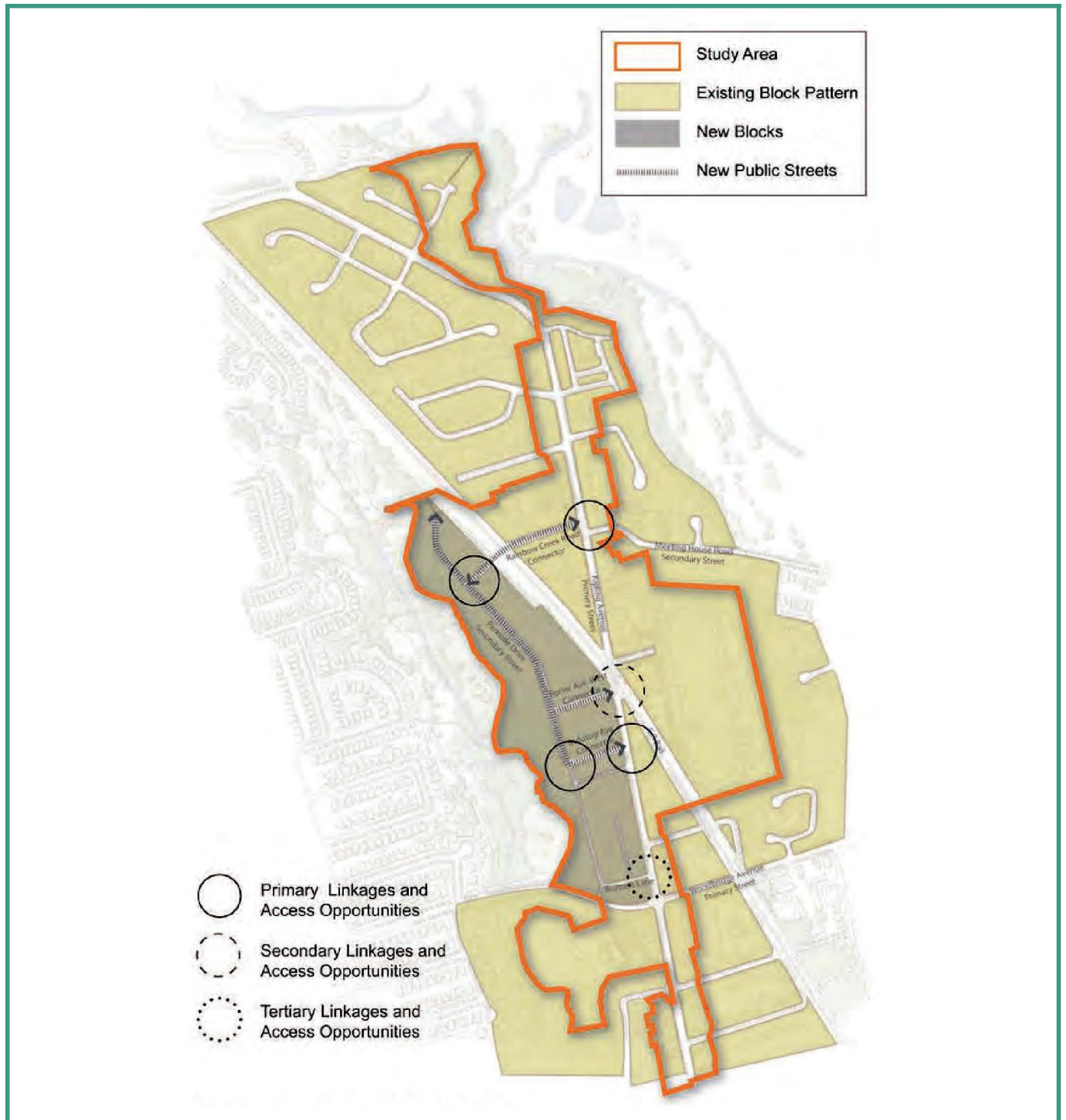


▼ Map 11.5.B  
Precincts and Gateways

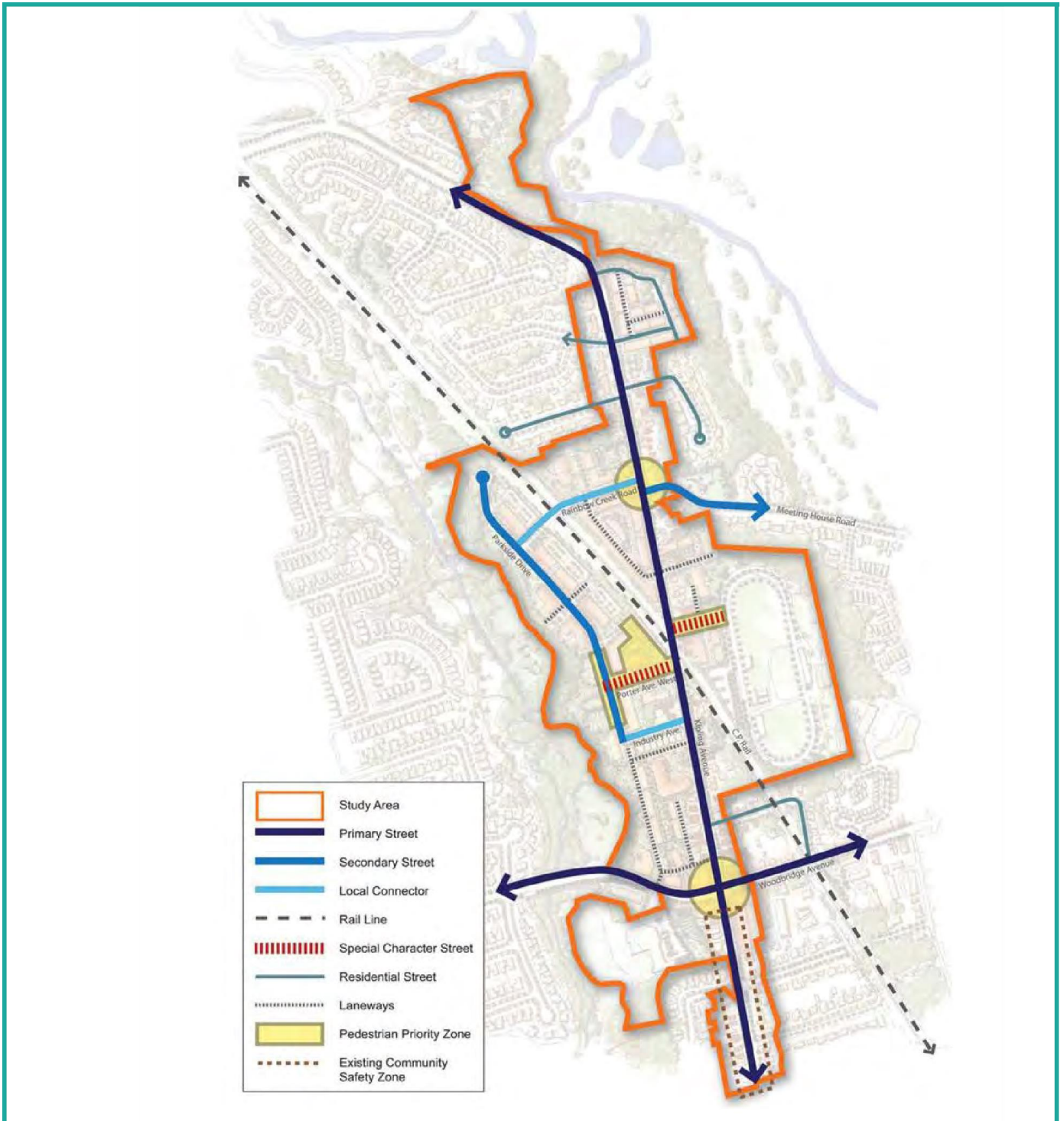




▼ Map 11.5.C  
 Block Pattern, Street Network and Linkages



▼ Map 11.5.D  
Street Network

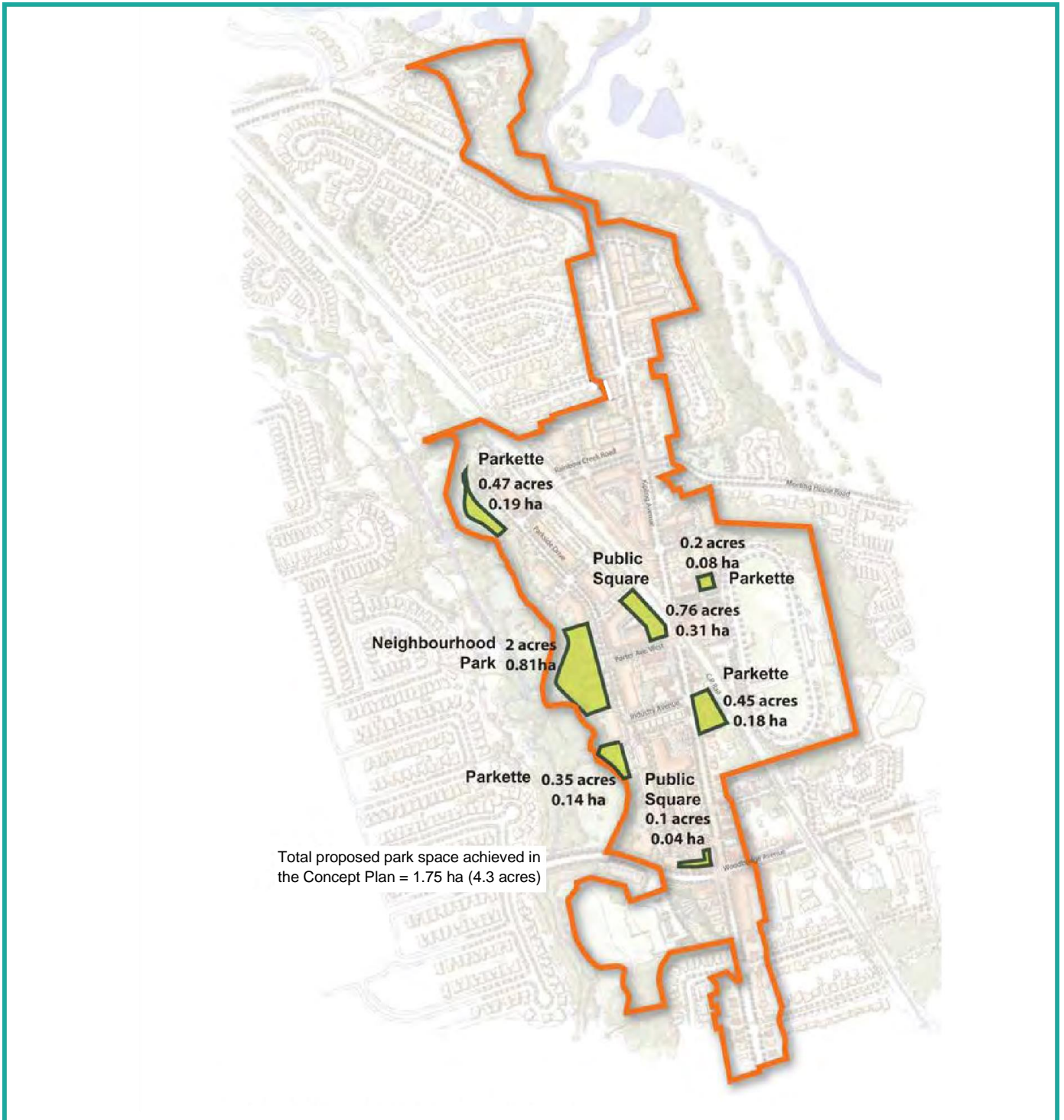




▼ Map 11.5.E  
Open Space Framework

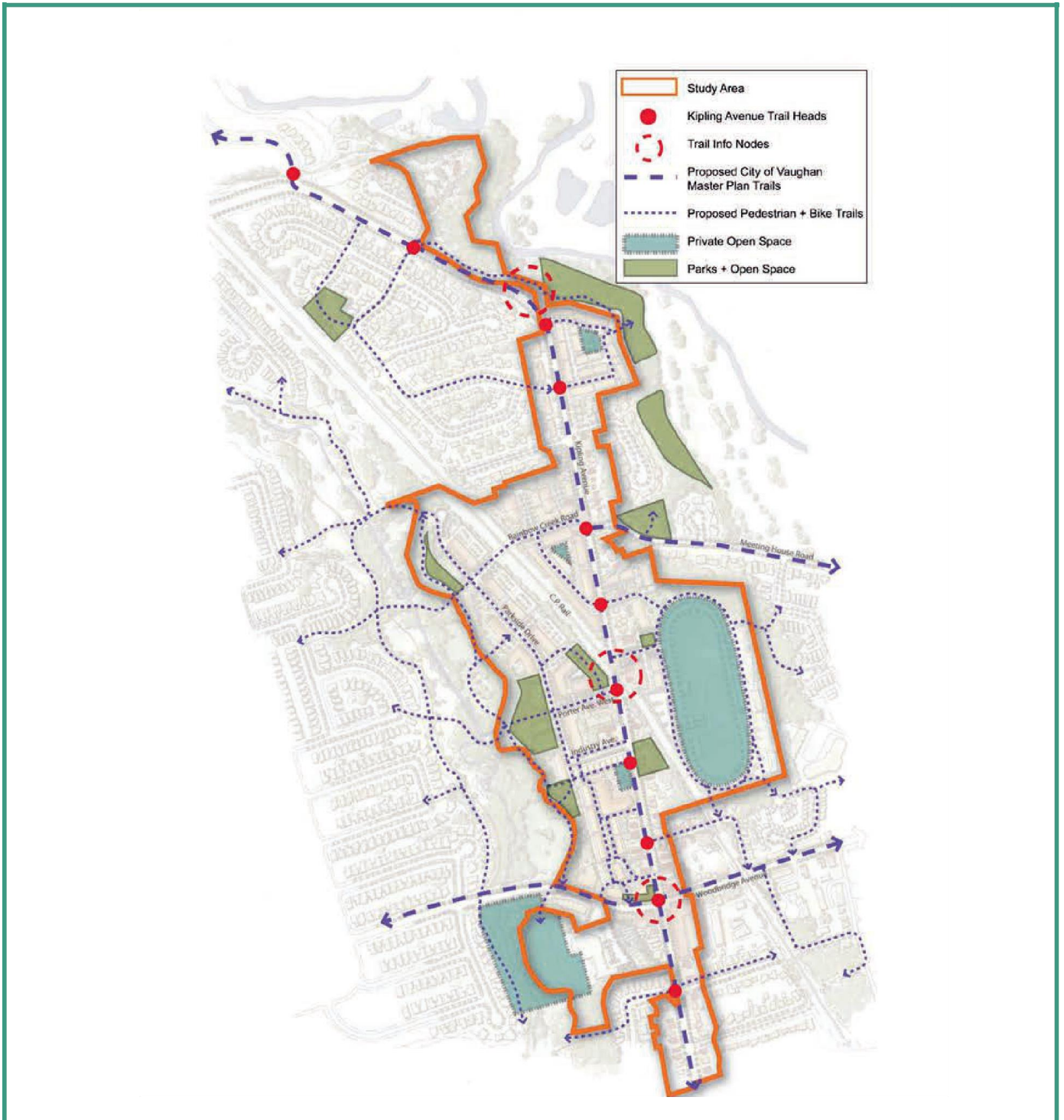


▼ Map 11.5.F  
Proposed Parks, Parkettes and Public Spaces

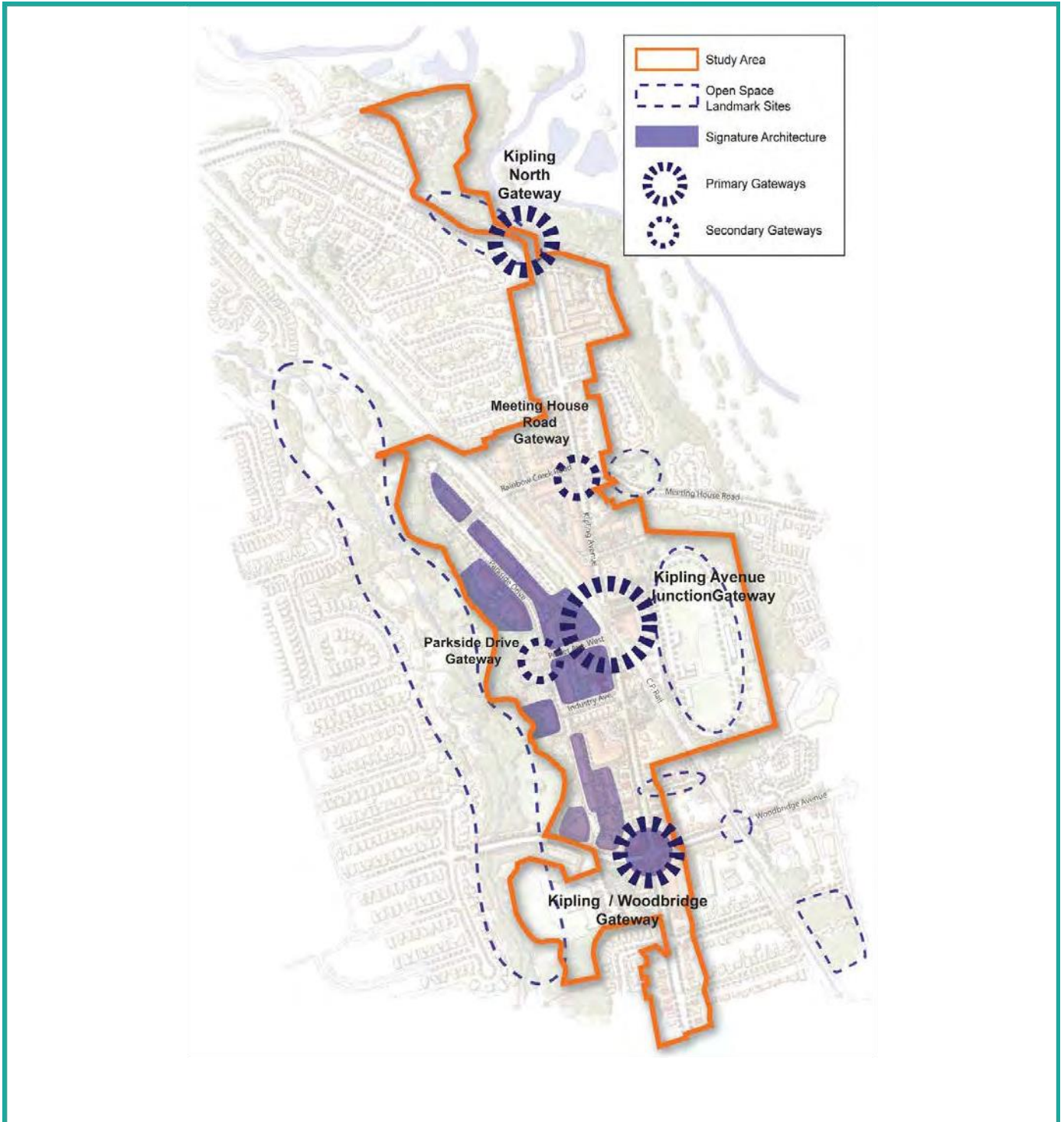




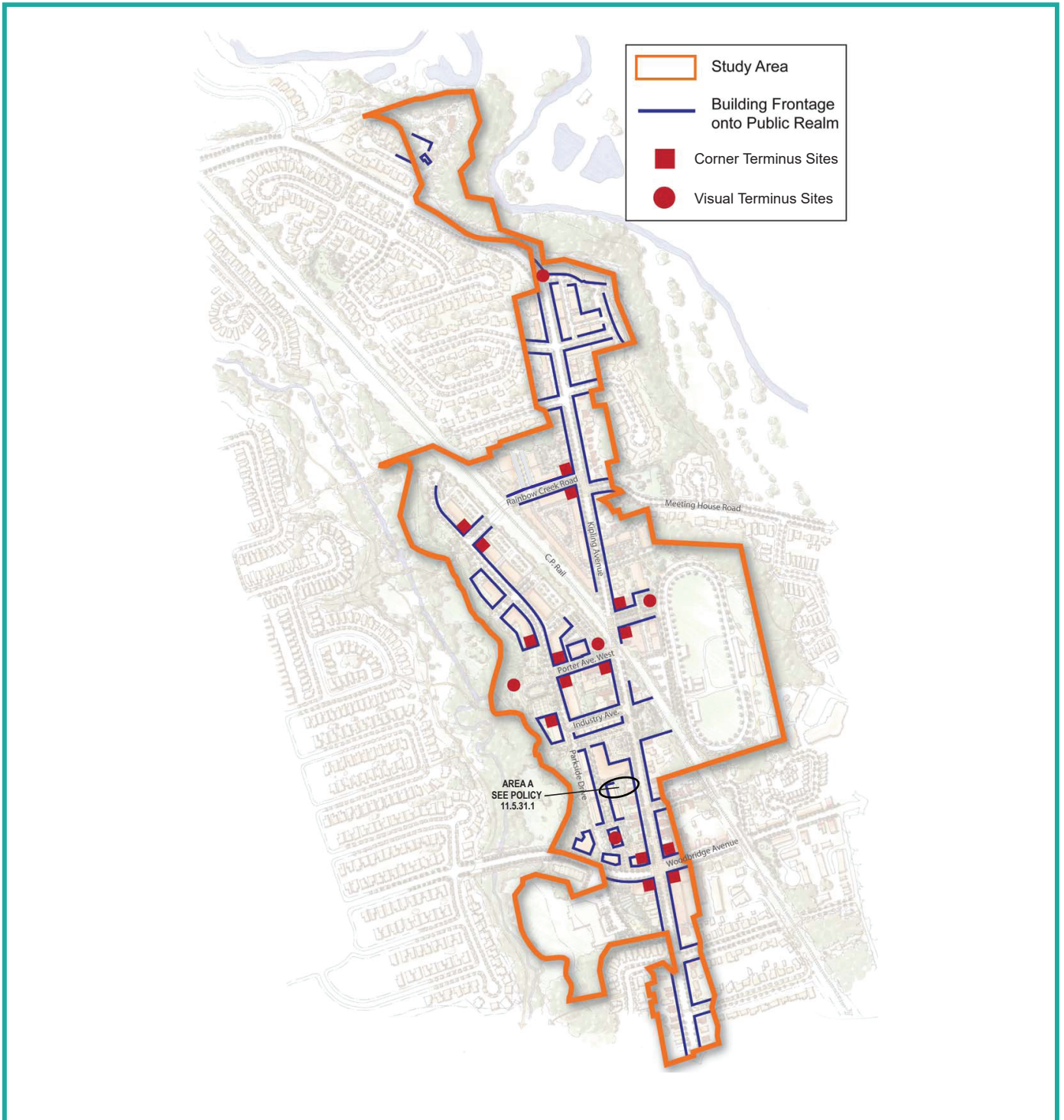
▼ Map 11.5.G  
Pedestrian and Bicycle Trail Network



▼ Map 11.5.H  
Landmark Sites and Gateways

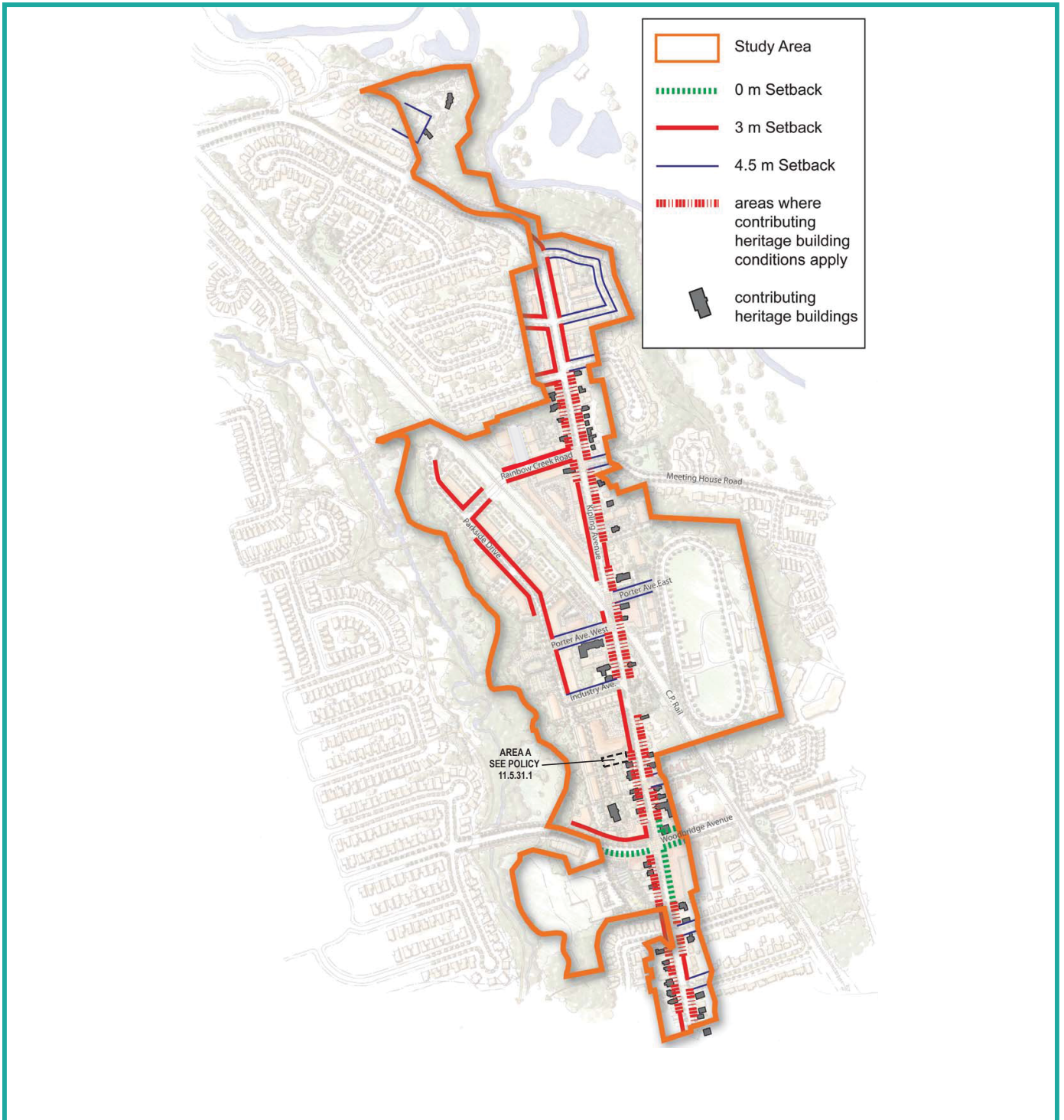


▼ Map 11.5.1  
Building Frontages, Orientation and, Corner and Terminus Sites

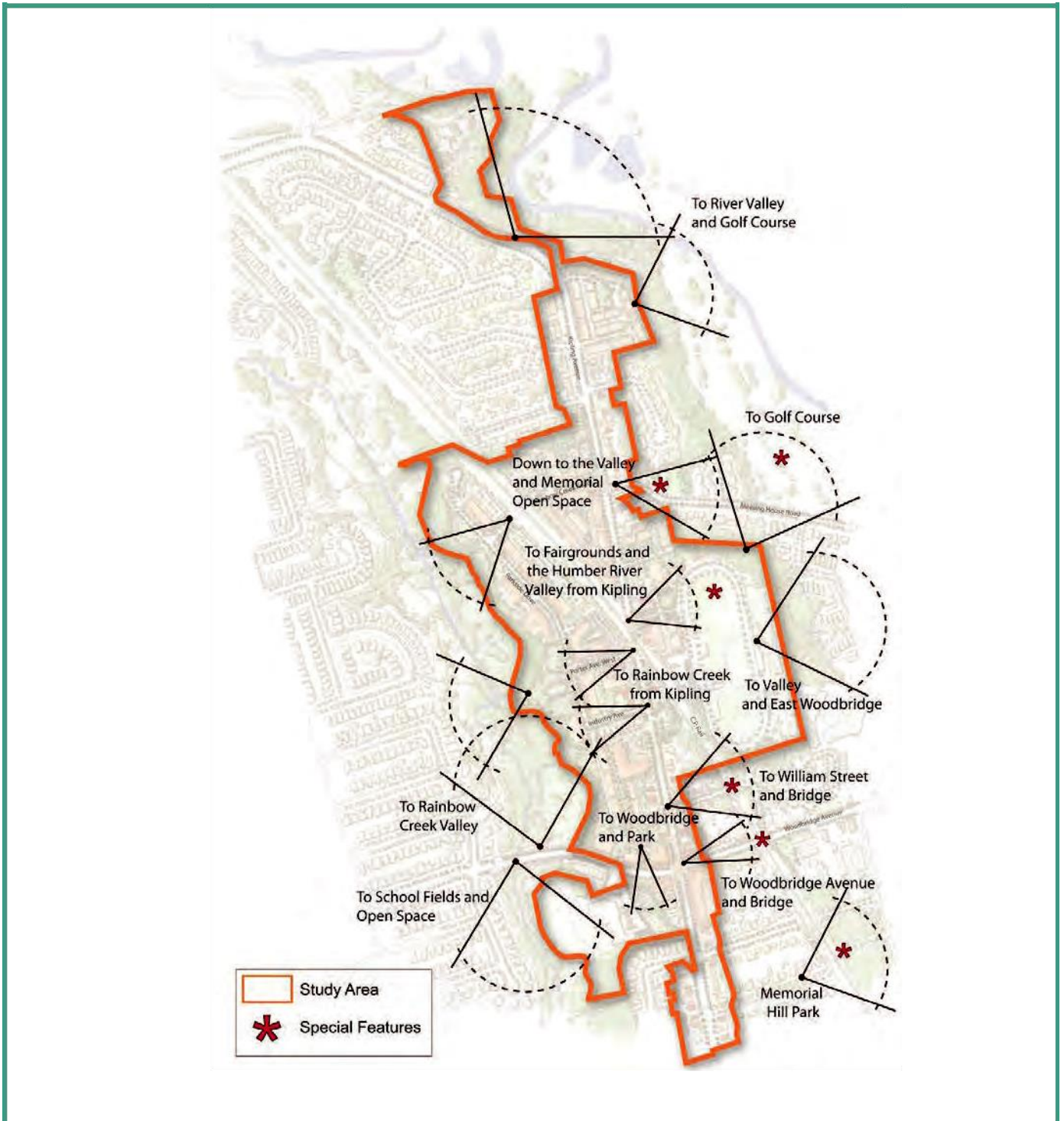




▼ Map 11.5.J  
Minimum Front-yard Building Setbacks



▼ Map 11.5.K  
Views and Landmark Features



Map 11.5.L





*Kipling Avenue - Identification of Heritage Resources*

The following is a list of properties within the Kipling Avenue Study Area considered to have cultural heritage value as evaluated through the Woodbridge Heritage Conservation District Study (HCD), and as contributing to the Woodbridge HCD Heritage Character:

7827 Kipling Avenue	8039
7833	8066
7844	8071
7845	8074
7848	8090
7856	8101
7866	8109
7872	8127
7880	8161
7883	8177
7895	8191
7909	8204
7912	8215
7924	8221
7934	8227
7938	8228
7955	8233
7971	8237
7974	8238
7981	8243
7984	8248
7985	8255
7988	8258
8000	8265
8001	3 Burton's Lane Industrial
8006	Building (image below)
8009	

The following two properties are within the Kipling Avenue Study Area (east of Gate House Court), but are not within the proposed Woodbridge Heritage Conservation District (HCD) boundary. These two properties, nonetheless, have been evaluated as contributing to the HCD Heritage Character:

- 8399 Kipling Avenue (Listed Heritage Property)
- 8265 Kipling Avenue (Identified in the Vaughan Heritage Inventory)

Map 11.5.M  
Kipling Avenue - Projected New Unit Counts and Density



*Kipling Avenue - Projected New Unit Counts and Density*

**Number of Units**

A	84
B	280
C	70
D	95
E	16
F	72
G	40
H	49
I	46
J	32
K	18
L	42
M	0
N	41
O	52
P	74
Q7	

For the purpose of calculating density, FLOOR SPACE INDEX (FSI) shall be the GROSS FLOOR AREA of a development divided by the GROSS SITE AREA. The deployment of allowable density must not result in buildings exceeding the maximum number of storeys or the maximum lot coverage.

The area included in the calculation of residential density shall include areas of a property reserved for setback requirements, the land for the buildings, private roads and driveways, parking areas and landscaping, amenity areas related to the specific development, but shall exclude all other lands such as natural conservation areas.

This is the maximum density and may not be achieved in all cases due to site constraints, zoning restrictions of adjacent properties, etc.

An average residential unit size of 1000 square feet (92.9 square metres) is used to calculate the number of units for apartments/condominiums. The townhouse units are calculated based on two to three storey standard townhouse frontages.

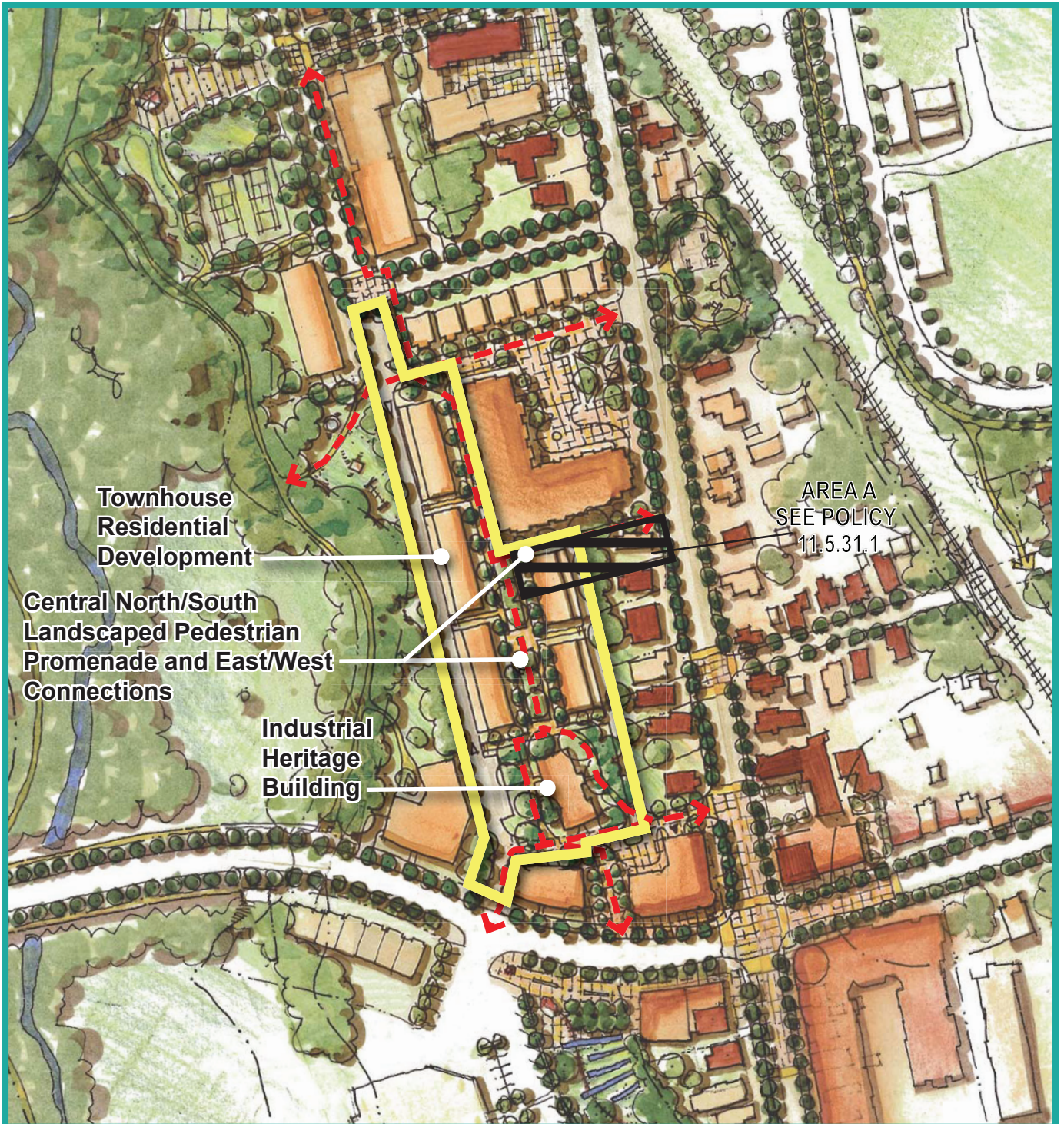
Total Units: 1022

Block	Block Area M2	Development M2				Total Units	FSI	Coverage	Observations
		Residential	Commercial	Parking garage	Total				
A	5,213	7,824		4,596	12,421	84	1.5	70%	
B	20,845	26,000	2,250	9,606	37,856	280	1.4	51%	
C	3,400	6,508			6,508	70	1.9	33%	
D	3,259	8,867			8,867	95	2.7	47%	
E	2,286	2,163			2,163	16	0.9	32%	Townhouses
F	3,323	6,653			6,653	72	2.0	50%	
G	3,982	3,800			3,800	41	1.0	24%	
H	12,554	6,804	951		7,755	49	0.6	35%	Townhouses + Apts.
I	2,051	4,540	908		5,448	49	2.7	44%	
J	1,547	2,948	406		3,353	32	2.2	54%	
K	988	1,677	559		2,236	18	2.3	57%	
L	4,440	3,397			3,397	42	0.8	38%	Townhouses
M	79,279		3,349	2,950	6,299	0	0.0	1%	Fairgrounds
N	5,913	4,483			4,483	41	0.8	26%	Townhouses
O	8,911	5,972			5,972	52	0.7	24%	Townhouses
P	6,242	6,920		2,896	9,816	74	1.1	42%	
Q	4,725	2,847			2,847	7	0.6	22%	Single Detached Houses
<b>TOTAL</b>	<b>168,957</b>	<b>101,404</b>	<b>8,422</b>	<b>20,048</b>	<b>129,874</b>	<b>1,022</b>			



Map 11.5.N

Kipling Avenue - The Rainbow Creek South Industrial Lands Low Density Residential Neighbourhood



# Appendix A: Streetscape Master Plan

## Detailed Streetscape Characteristics

The general characteristics of each street section is outlined in Section 11.5.3: Block Pattern, Street Network and Linkages. The Detailed Streetscape Guidelines is based on a general concept plan and is meant to be considered in conjunction with the Built Form Framework Guidelines.

streets for the area, which may require additional detail design review and input by various agencies such as the Vaughan Fire Department (VFRS), and Public Works and Emergency Services, to ensure that consideration for road safety, servicing and access are being met.

### **Kipling Avenue North - Primary Main Street**

- R.O.W. = 26 m
- Two travel lanes = 3.75 m (to accommodate buses)
- Parking lane = 2.5 m (on east side of street)
- Two bicycle lanes = 1.5 m (one on each side of the street)
- Average pavement width = 13 m
- Boulevard width = 6.5 m (on both sides of the street)
- Sidewalks = 1.5 m (on both sides of the street)
- Minimum setback for buildings = 3 m

### **Kipling Avenue South - Primary Main Street**

- R.O.W. = 26 m
- Two travel lanes = 3.3 m (one as a left turn lane at intersection)
- Two bus and travel lanes = 3.75 m
- On-street Parking = (off peak parking use of travel lane on east side only)
- Left turning lane = at Kipling Avenue and the Highway 7 intersection
- Two Bicycle lanes = 1.45 m (one on each side of the street, the east lane to have a dedicated curb edge or rumble strip to the adjacent travel lane)
- Average pavement width = 17.0 m
- Boulevard width = 4.5 m (on both sides of the street)
- Sidewalks = 1.5 m (on both sides of the street)
- Minimum setback for buildings = 3 m

### **Woodbridge Avenue East - Primary Main Street**

- R.O.W. = 20 m
- One Travel lane = 3.3 m (travel and turning lane)
- Two Bus lanes = 3.75 m (shared bus, bike and travel lane)
- Parking lane = 2.5 m (lay-by parking beyond intersection)
- Left turning lane = at Kipling Avenue
- Average pavement width = 10.8 m (15.8 m including lay-by parking east of intersection)
- Minimum setback for buildings = 0 m - 3 m (3 m minimum setback for the north/west corner of Kipling Avenue and Woodbridge Avenue)
- Boulevard width = 4.0 m (on north side of the street), 5.2 m (on south side of street, including bump-out)
- Sidewalks = 1.5 m (on both sides of the street)

### **Woodbridge Avenue West - Primary Main Street**

- R.O.W. = 26 m
- Two Travel lanes = 3.3 m (travel and dedicated turning lane)
- Two Bus lanes = 3.75 m (shared bus, bike and travel lane)
- Parking lane = 2.5 m (lay-by parking beyond intersection)
- Left turning lane = at Kipling Avenue
- Average pavement width = 14.1 m (19.1 m including lay-by parking east of intersection)
- Boulevard width = 5.95 m (on both sides of the street)
- Sidewalks = 1.5 m (on both sides of the street)
- Minimum setback for buildings = 0 m - 3 m (3 m minimum setback for the north/west corner of Kipling Avenue and Woodbridge Avenue)

### **Parkside Drive - Secondary Street**

- R.O.W. = 20 m
- Two Travel lanes = 3.25 m
- Parking lane = 2.5 m
- Left turning lane = none
- Average pavement width = 9 m
- Boulevard width = 5.50 m (on both sides of the street)



- Sidewalks = 3.5 m zone (on both sides of the street with integrated street tree planting and landscaping)
- Minimum setback for buildings = 3 m

### **Rainbow Creek Road - Local Connector**

- R.O.W. = 20 m
- Two Travel lanes = 3.25 m
- Parking lane = 2.5 m
- Left turning lane = none
- Average pavement width = 9 m
- Boulevard width = 5.50 m (on both sides of the street)
- Sidewalks = 3.5 m zone (on both sides of the street with integrated street tree planting and landscaping)
- Minimum setback for buildings = 3 m

### **Industry Avenue - Local Connector**

- R.O.W. = 17.5 m
- Two travel lanes = 3.0 m
- Parking lane = 2.5 m on north side
- Left turning lane = none
- Average pavement width = 8.5 m
- Minimum setback for buildings = 3 m
- Sidewalks = 1.5 m (on north side of the street)

### **Porter Avenue East - Special Character Street**

- R.O.W. = 20 m
- Two travel lanes = 3.5 m
- Parking lanes = none
- Left turning lane = none
- Average pavement width = 7.0 m
- Sidewalks = 8.5 m pedestrian promenade on the south side (2.5 m and 3.0 m double sidewalk with integrated pedestrian furnishings), and a 4.5 m promenade on the north side (2.5 m and 2.0 m double sidewalk)
- Minimum setback for buildings = 4.5 m

### **Porter Avenue West - Special Character Street**

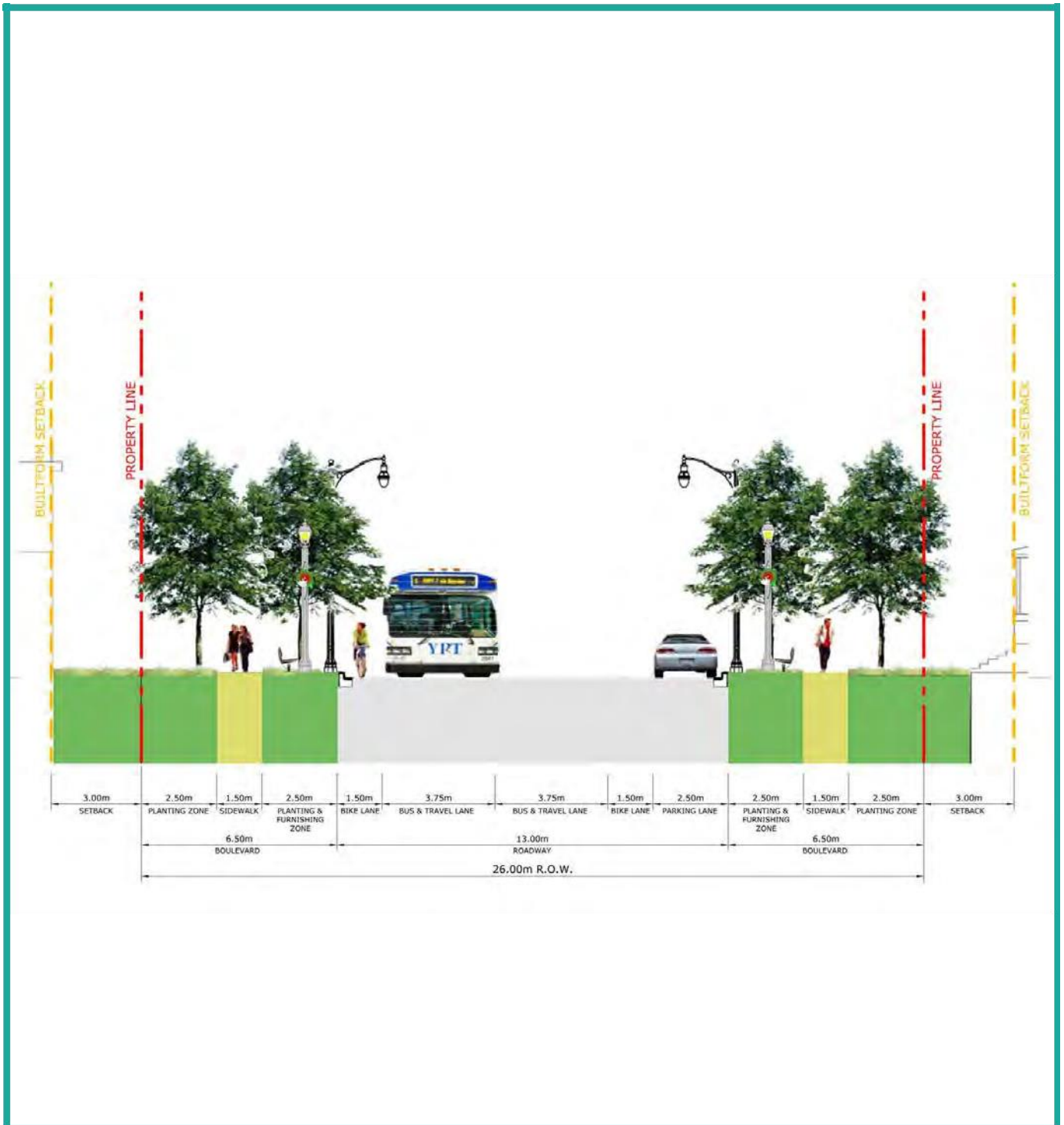
- R.O.W. = 17.5 m
- One travel lane and lay-by parking = 6.0 m (one way direction east to Kipling Avenue, and lay-by parking on north side)
- Left turning lane = none
- Average pavement width = 6.0 m (including lay-by parking)
- Sidewalks = 2.5 m on the north side of the street, and 1.5 m on the south side where possible, (considering the existing heritage church pinch point)
- Minimum setback for buildings = 4.5 m (with the exception of the existing heritage church 1.5 m pinch point to the R.O.W.)

### **Residential Streets - Typical**

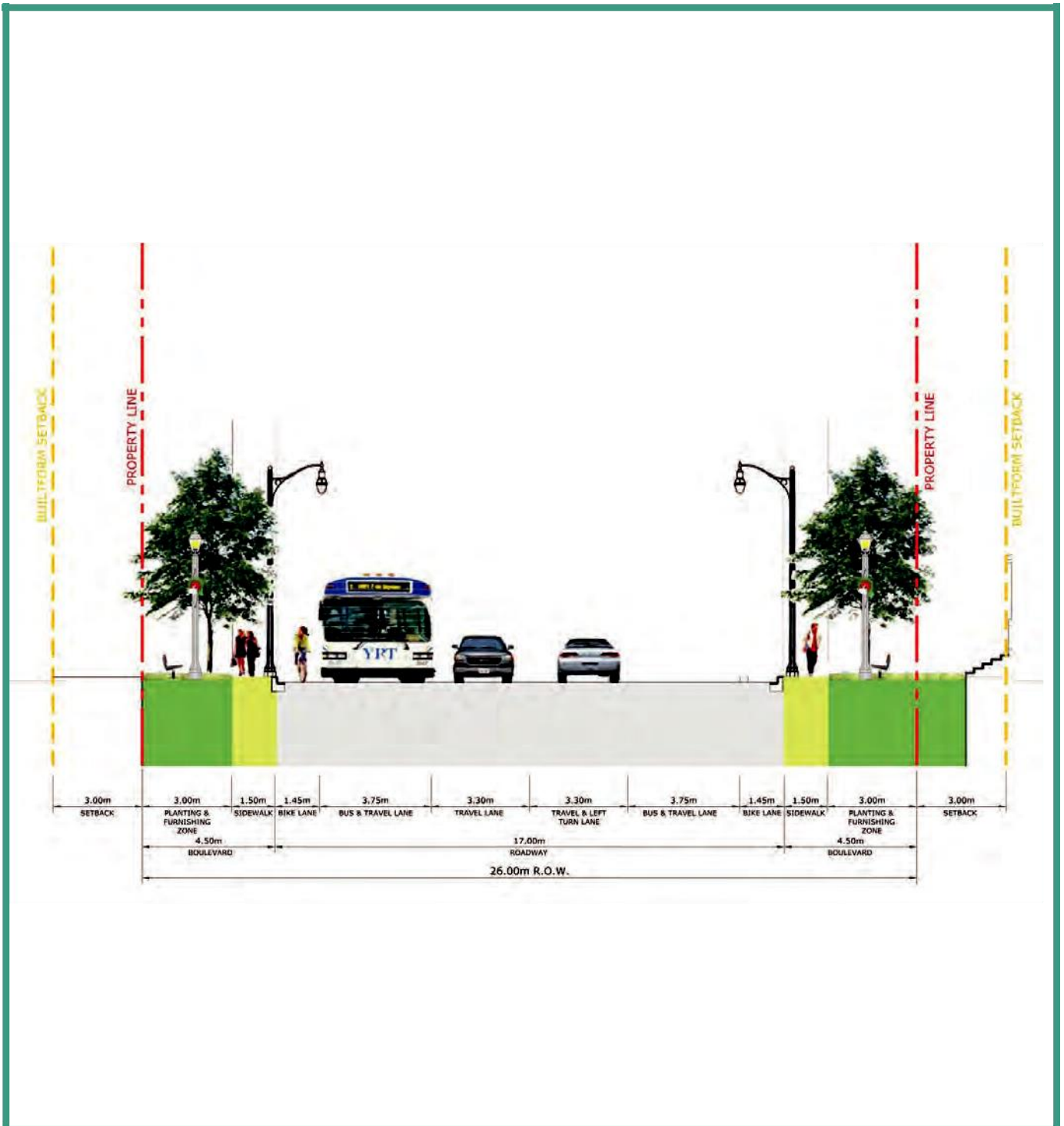
- R.O.W. = 20 m
- Two travel lanes = 3.0 m
- Parking lane = 2.5 m
- Left turning lane = none
- Average pavement width = 8.5 m
- Minimum setback for buildings = 4.5 m
- Sidewalks = 2.0 m (on both sides of the street)



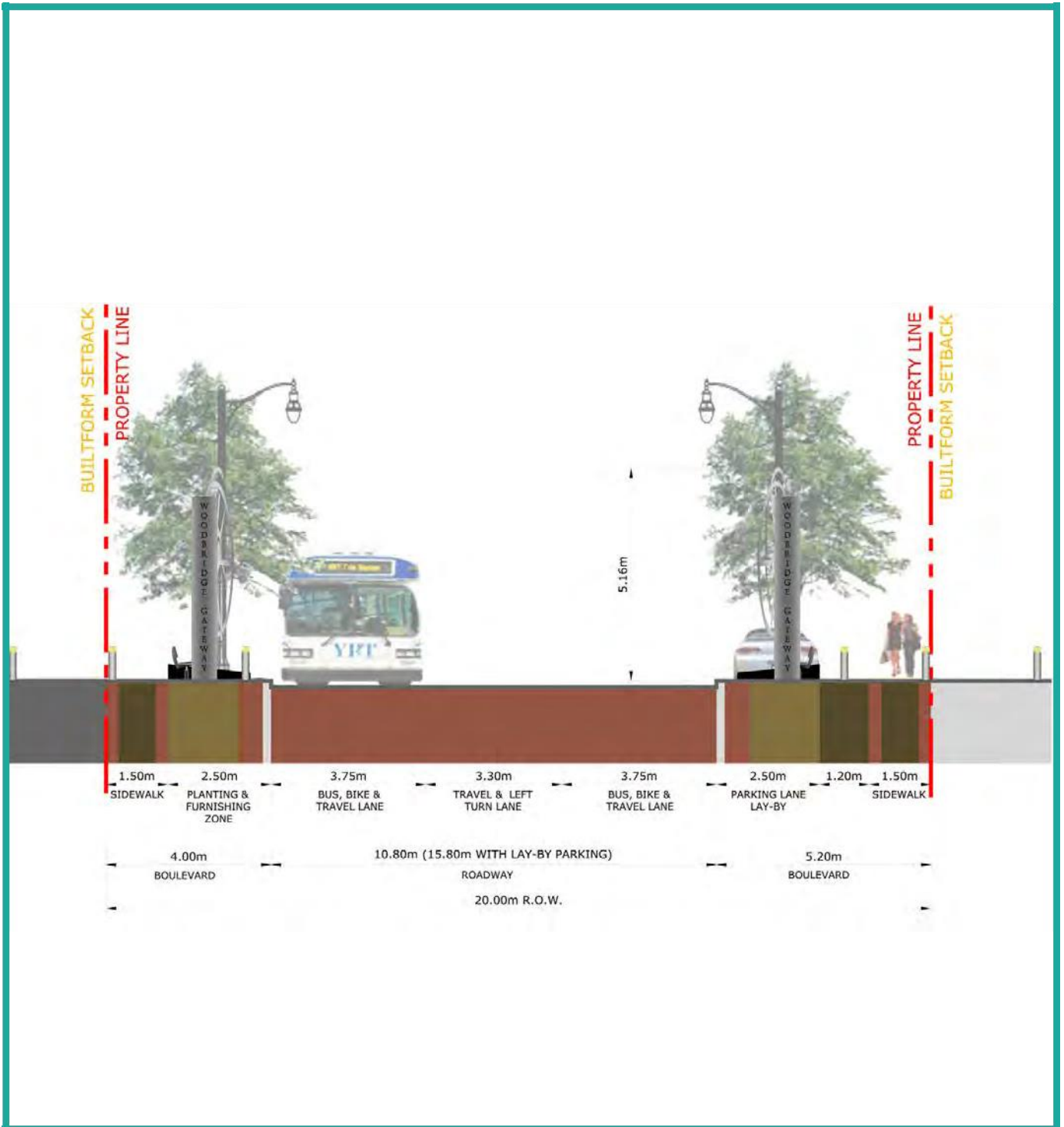
▼ Figure 11.5.A  
 Kipling Avenue - Street Section - Kipling Avenue North - Primary Main Street - View North



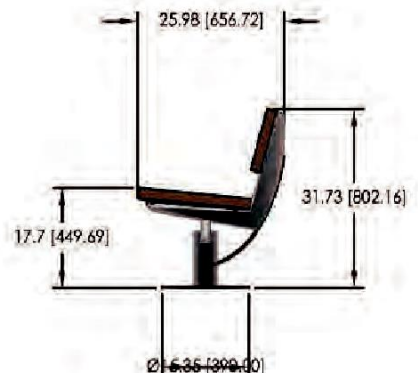
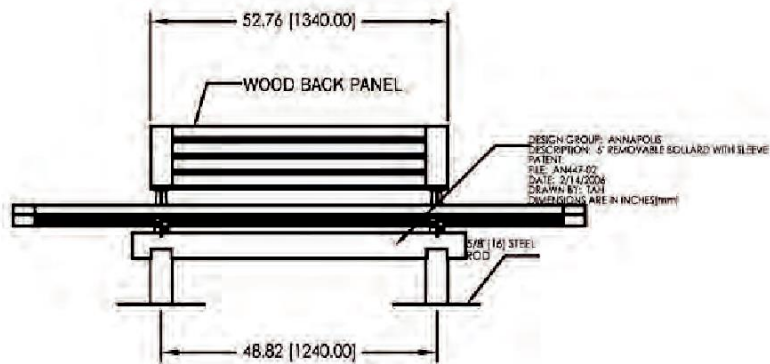
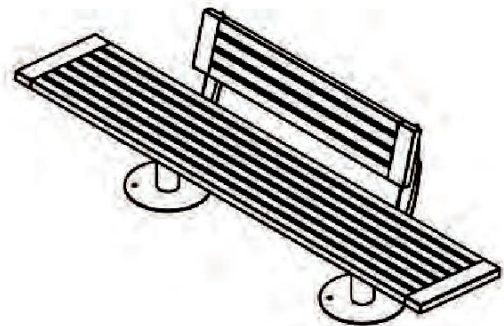
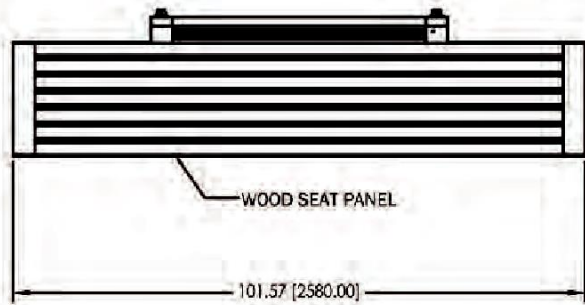
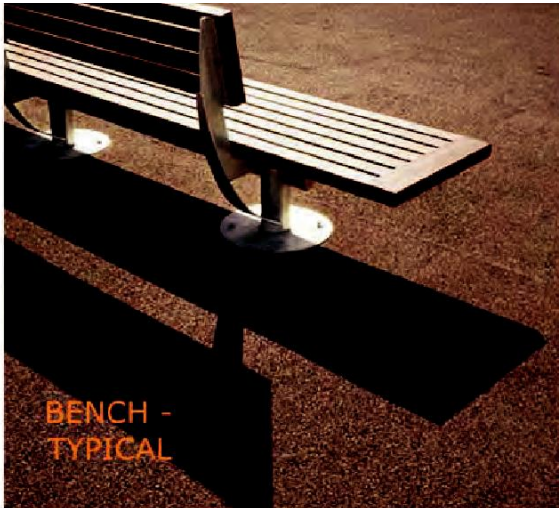
▼ Figure 11.5.B  
 Kipling Avenue - Street Section - Kipling Avenue South - Primary Main Street - View North



▼ Figure 11.5.C  
 Kipling Avenue - Street Section/Elevation - Woodbridge Avenue East - Primary Main Street and Gateway Details - View East

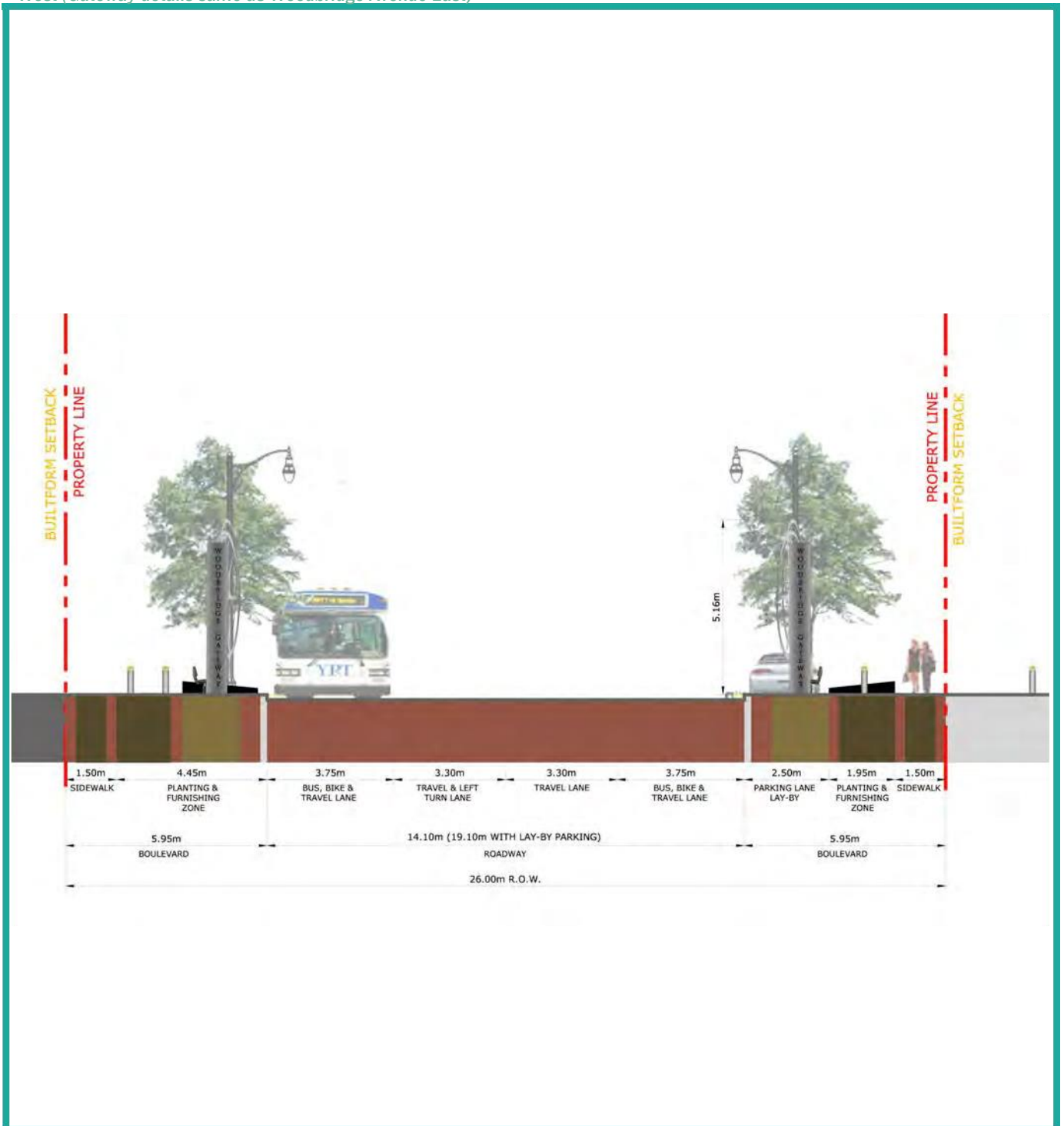


Kipling Avenue - Street Section/Elevation - Woodbridge Avenue East - Primary Main Street and Gateway Details - View East



DESIGN GROUP: ANNAPOUS  
 DESCRIPTION: 6' REMOVABLE BOLLARD WITH SLEEVE  
 PATENT  
 FILE: ANNA07-02  
 DATE: 2/14/2004  
 DRAWN BY: TAJ  
 DIMENSIONS ARE IN INCHES(MM)

▼ Figure 11.5.D  
 Kipling Avenue - Street Section/Elevation - Woodbridge Avenue West - Primary Main Street - View  
 West (Gateway details same as Woodbridge Avenue East)

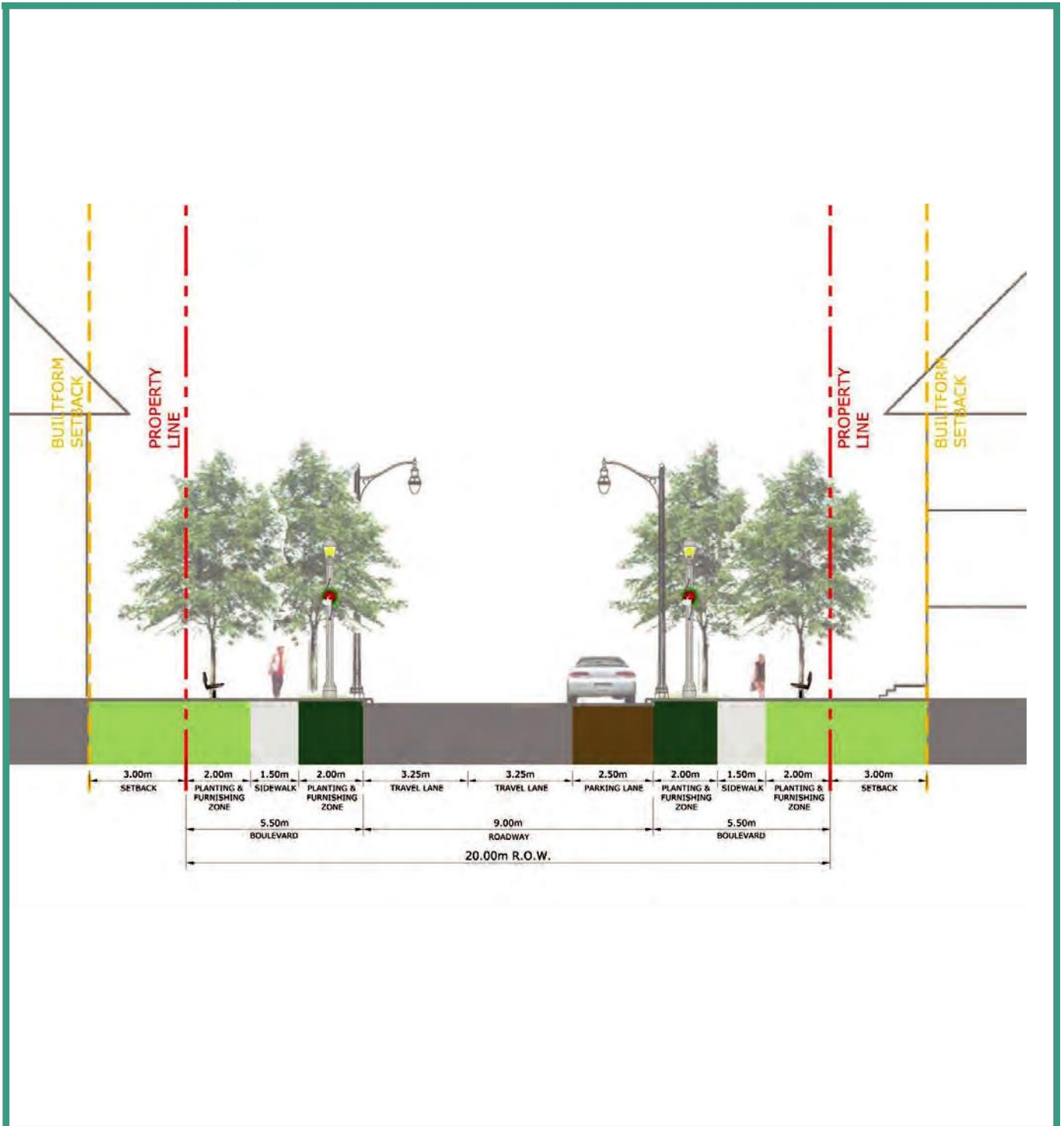




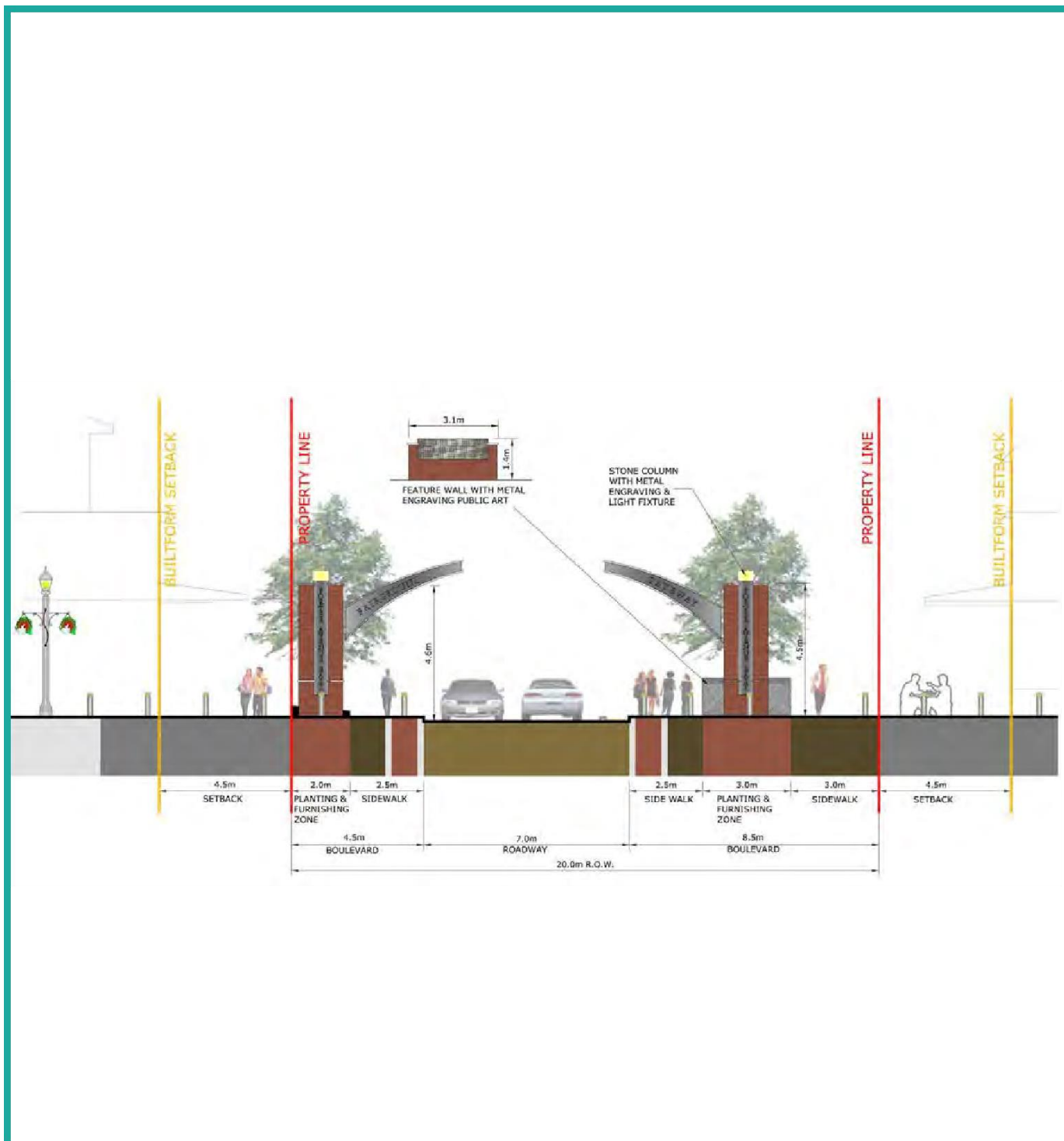
▼ Figure 11.5.E

Kipling Avenue - Street Section/Elevation - Parkside Drive - View West

(Rainbow Creek Road, although considered a local connector, has the same cross section as Parkside Drive)

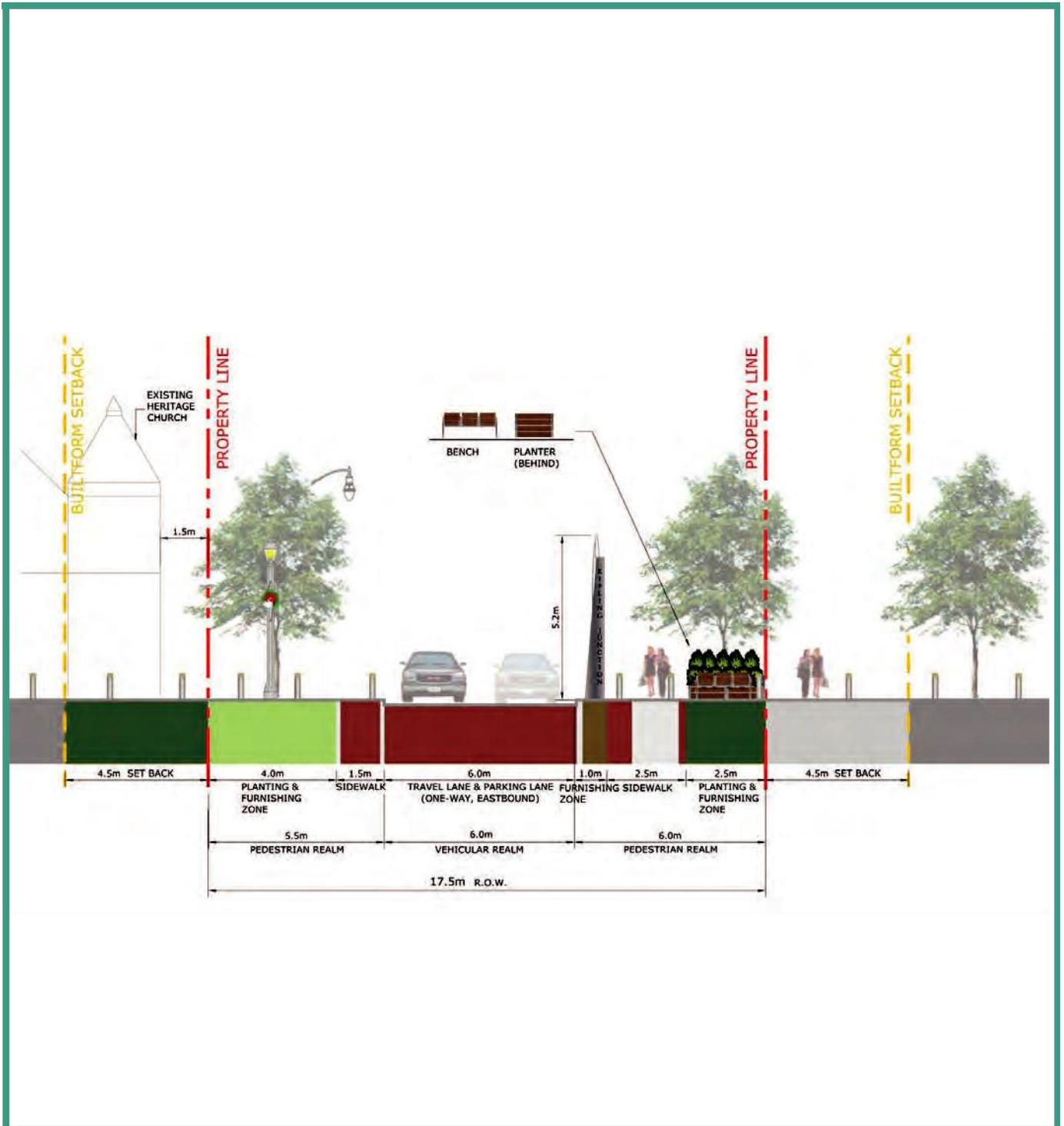


▼ Figure 11.5.F  
 Kipling Avenue - Street Section/Elevation - Porter Avenue East - Special Character Street - View East



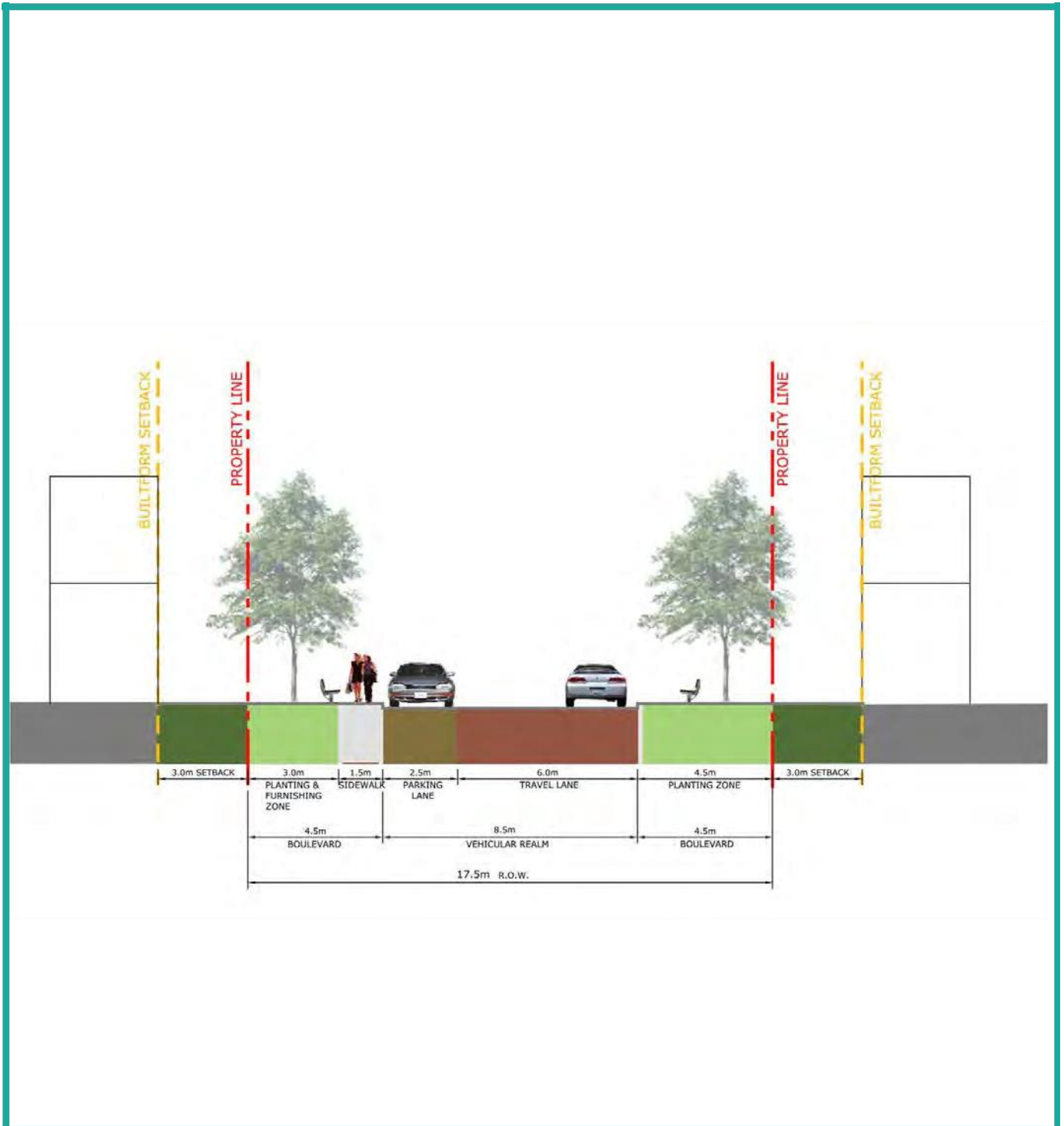
▼ Figure 11.5.G

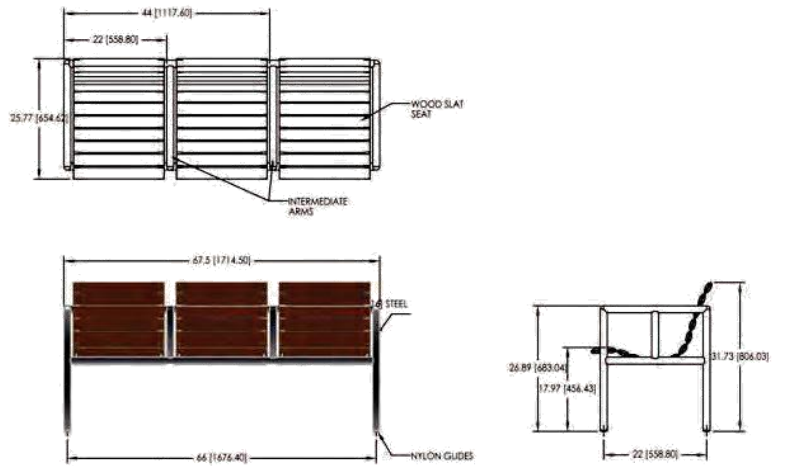
Kipling Avenue - Street Section/Elevation - Porter Avenue West - Special Character Street - View West



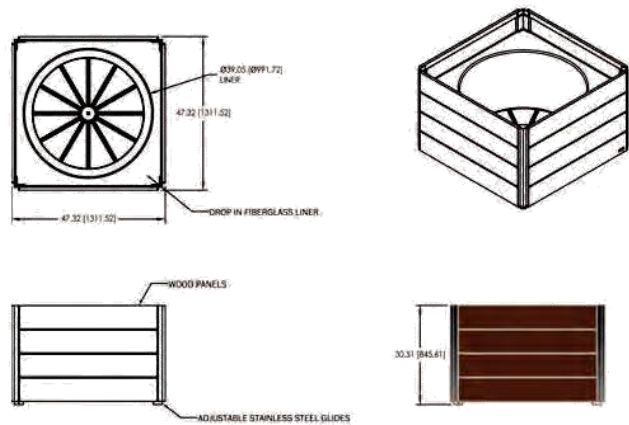


▼ Figure 11.5.H  
 Kipling Avenue - Street Section/Elevation - Industry Avenue - Local Connector - View East



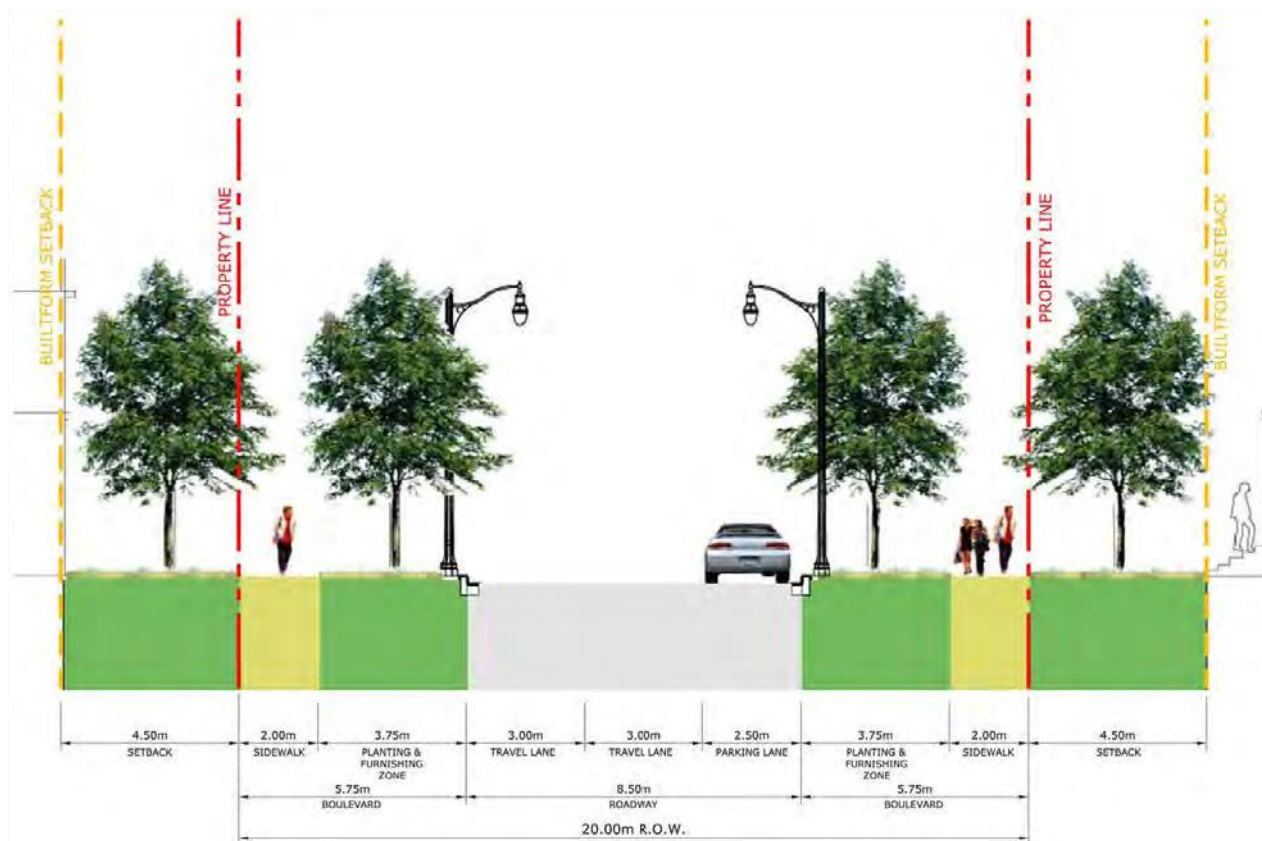


**BENCH - KIPLING JUNCTION TYPICAL**



**PLANTER - KIPLING JUNCTION TYPICAL**

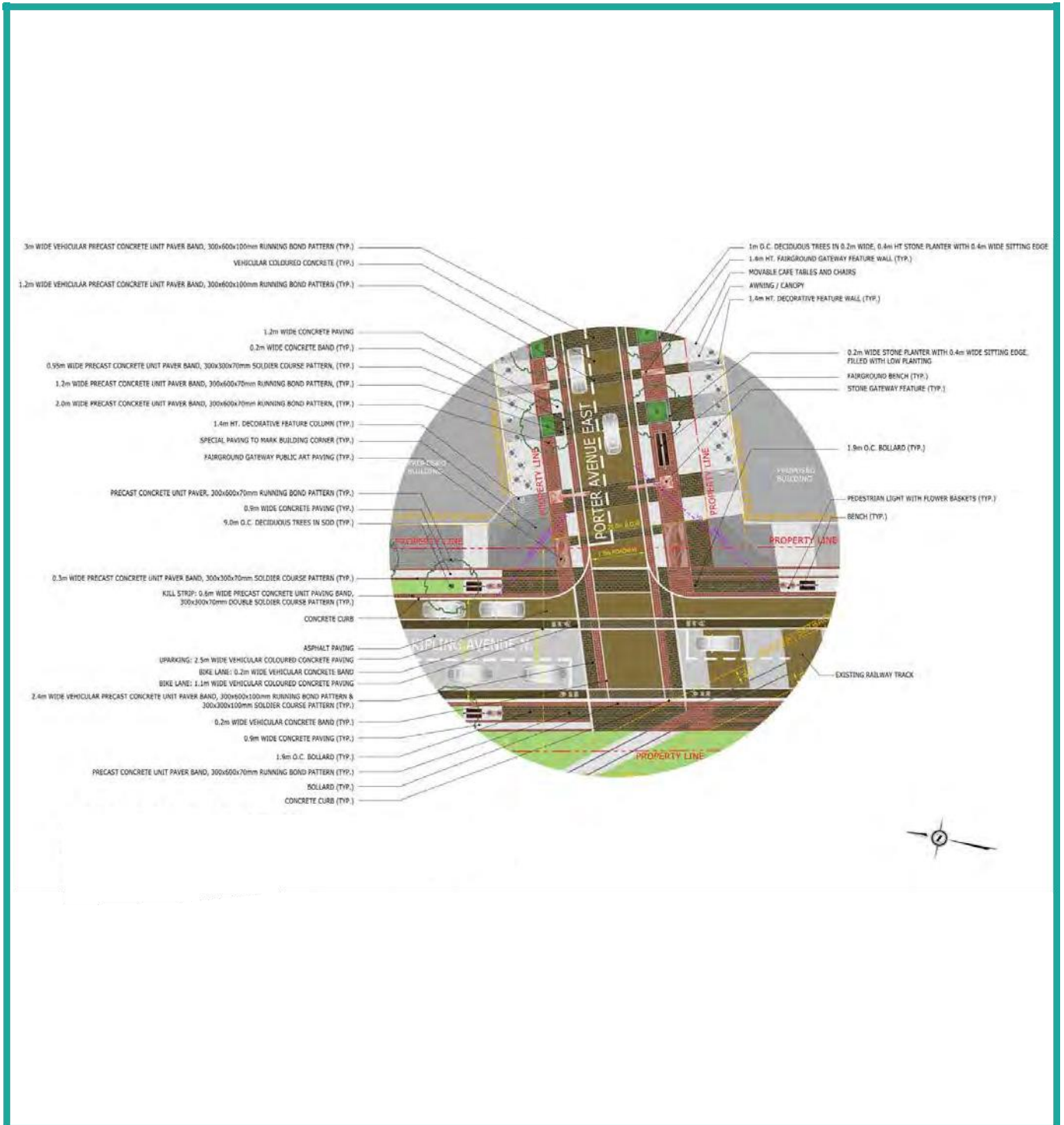
▼ Figure 11.5.1  
 Kipling Avenue - Street Section/Elevation - Residential Street - Typical







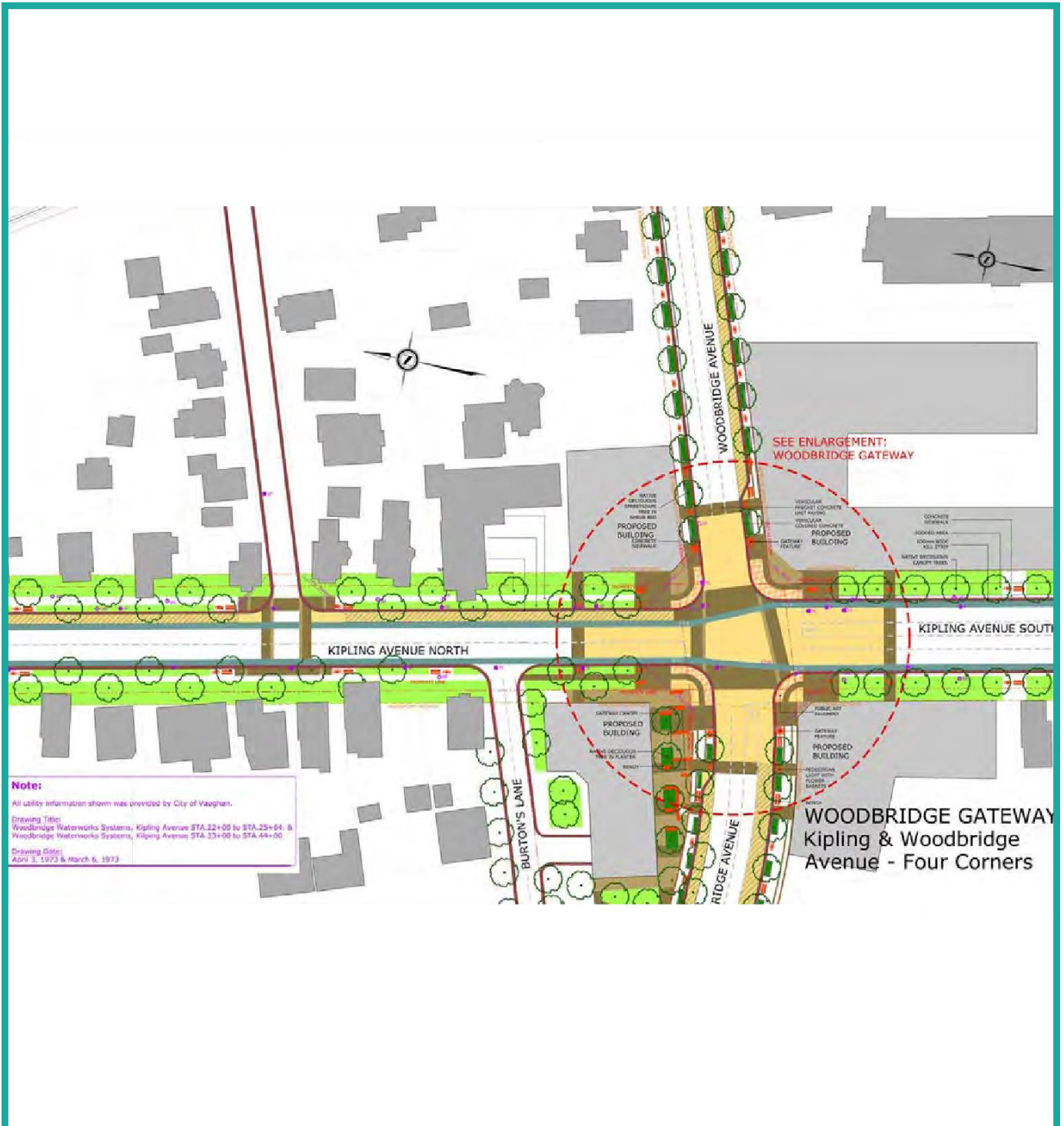
▼ Figure 11.5.K  
 Kipling Avenue - Porter Avenue East - The Faiground Gateway



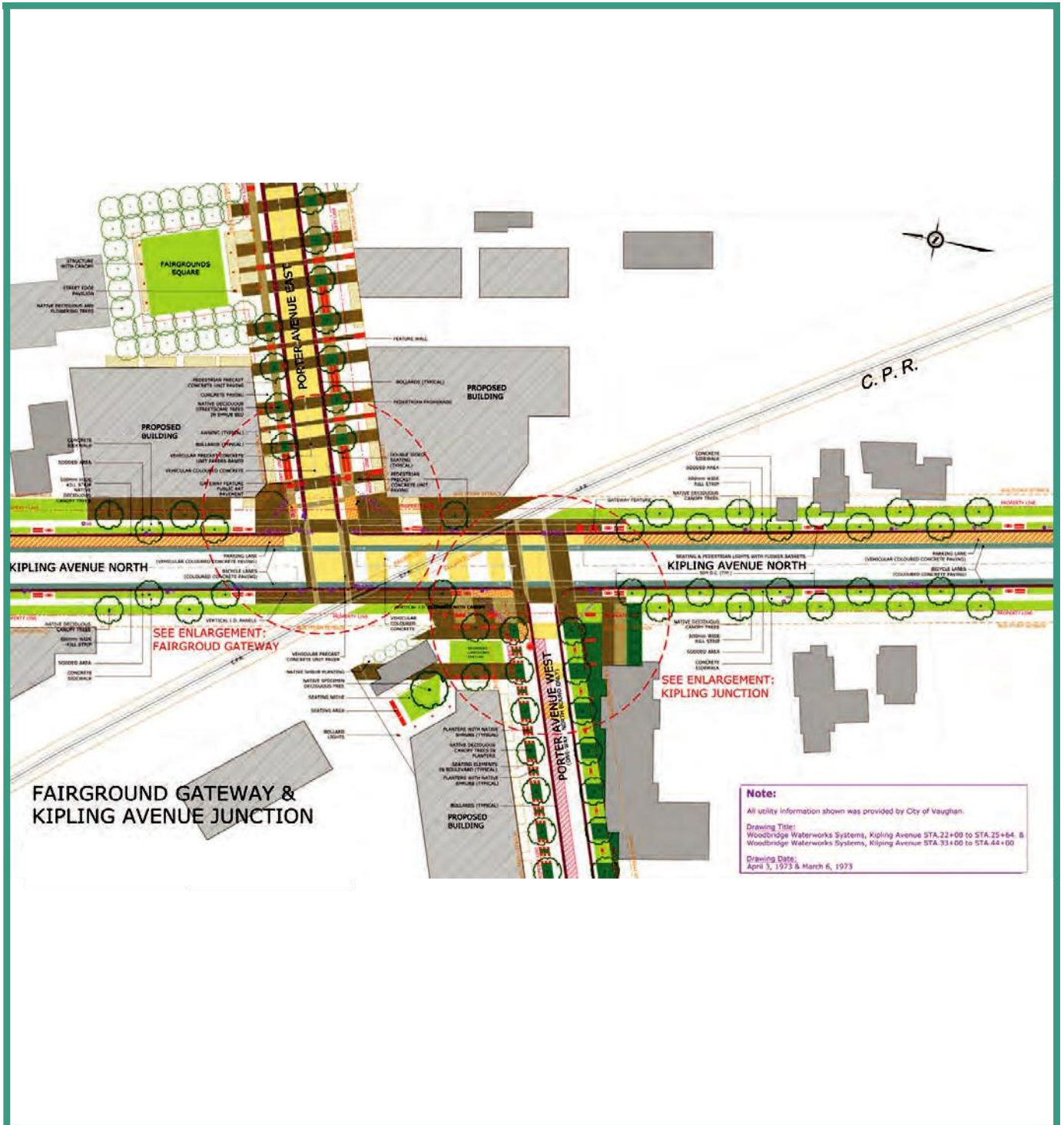




▼ Figure 11.5.M  
 Kipling Avenue - Woodbridge Avenue and Kipling Avenue Gateway Master Plan



▼ Figure 11.5.N  
 Kipling Avenue - Porter Avenue East - The Junction Master Plan



# Appendix B: Transitions of New Buildings to Heritage Resources

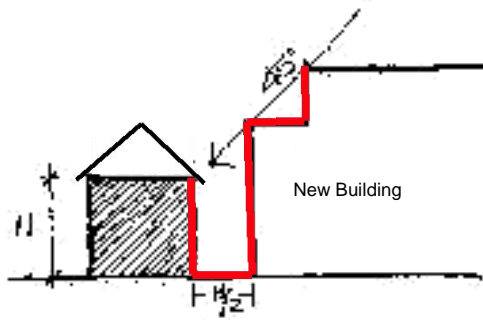


Diagram A

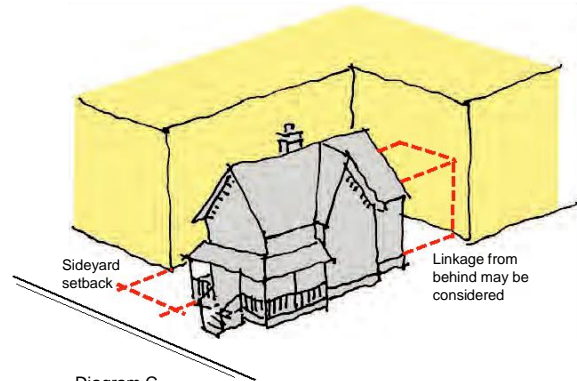


Diagram C

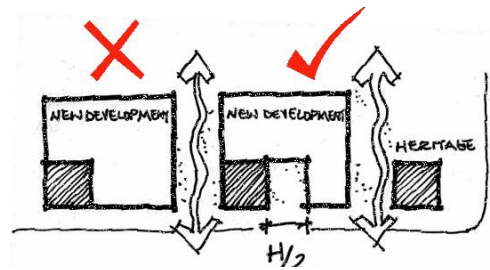


Diagram D

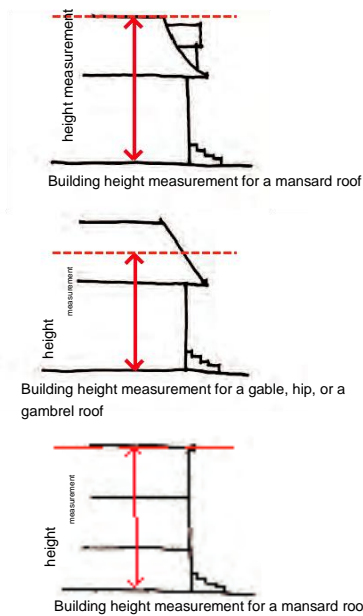


Diagram B

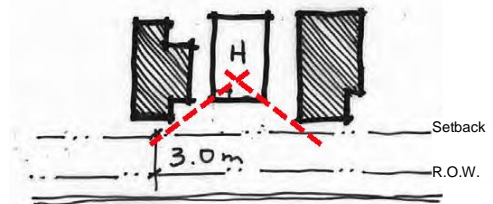


Diagram E

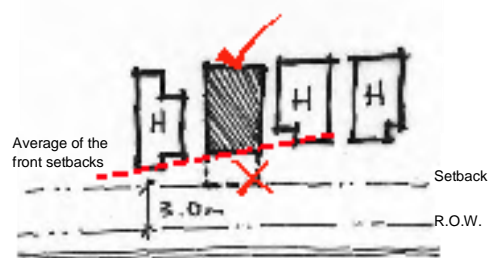


Diagram F